

# WALTON BOULEVARD AND MEDICAL CENTER PARKWAY IMPROVEMENTS

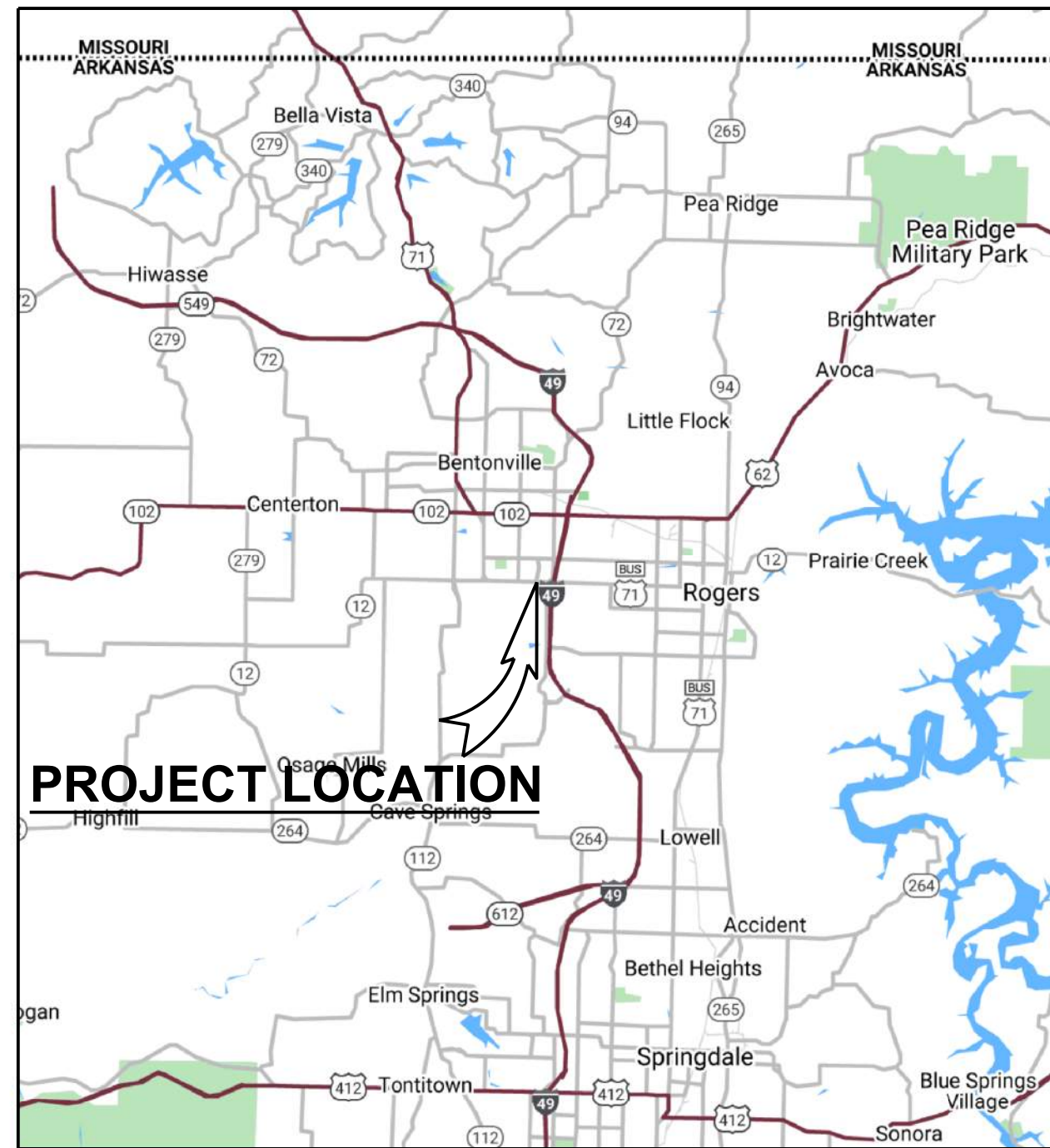
## CITY OF BENTONVILLE

### BENTONVILLE, ARKANSAS

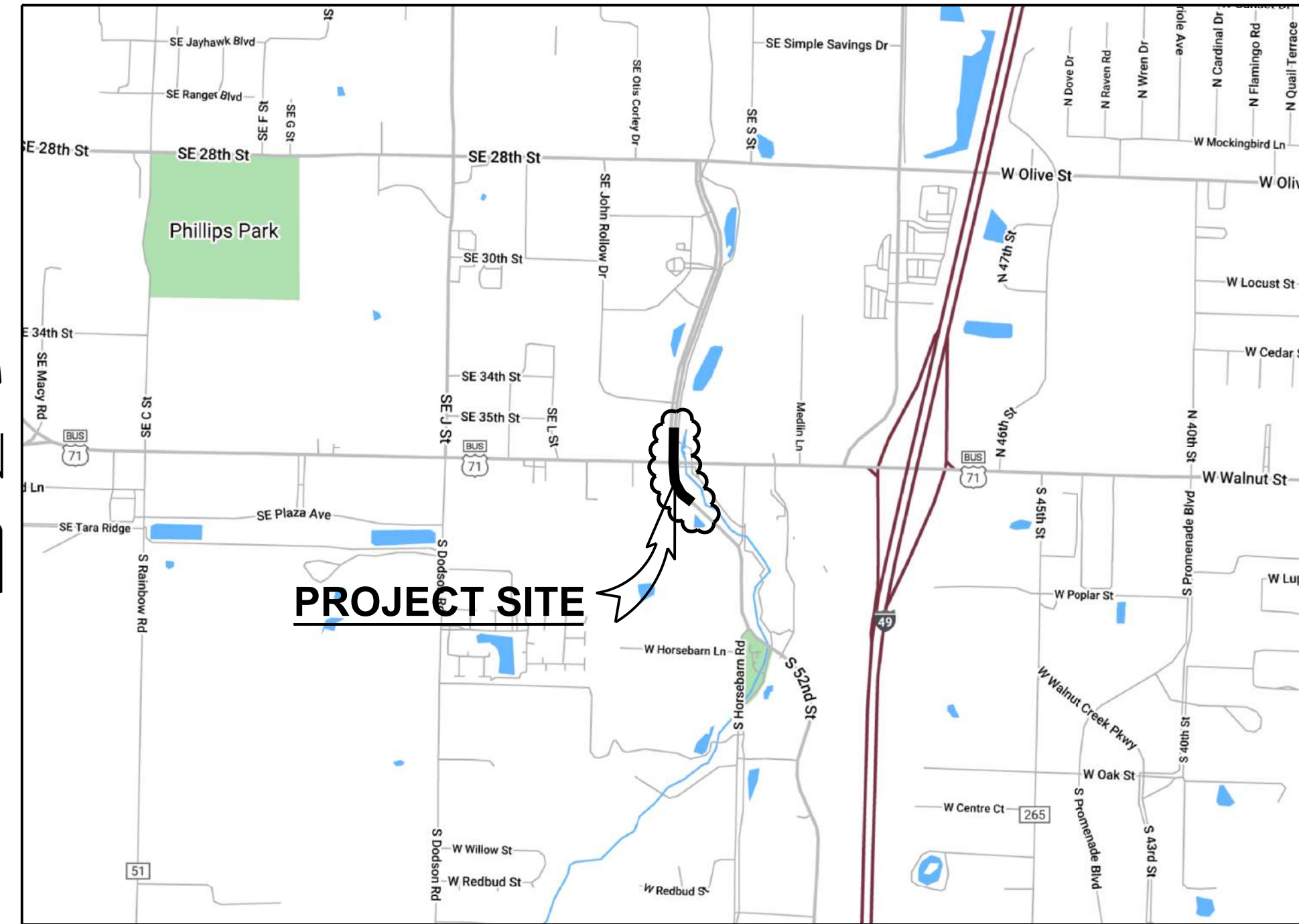


© 2022 GARVER, LLC  
 THIS DOCUMENT, ALONG WITH THE IDEAS AND DESIGNS CONVEYED HEREIN, SHALL BE CONSIDERED INSTRUMENTS OF PROFESSIONAL SERVICE AND ARE PROPERTY OF GARVER, LLC. ANY USE, REPRODUCTION, OR DISTRIBUTION OF THIS DOCUMENT, ALONG WITH THE IDEAS AND DESIGN CONTAINED HEREIN, IS PROHIBITED UNLESS AUTHORIZED IN WRITING BY GARVER, LLC OR EXPLICITLY ALLOWED IN THE GOVERNING PROFESSIONAL SERVICES AGREEMENT FOR THIS WORK.

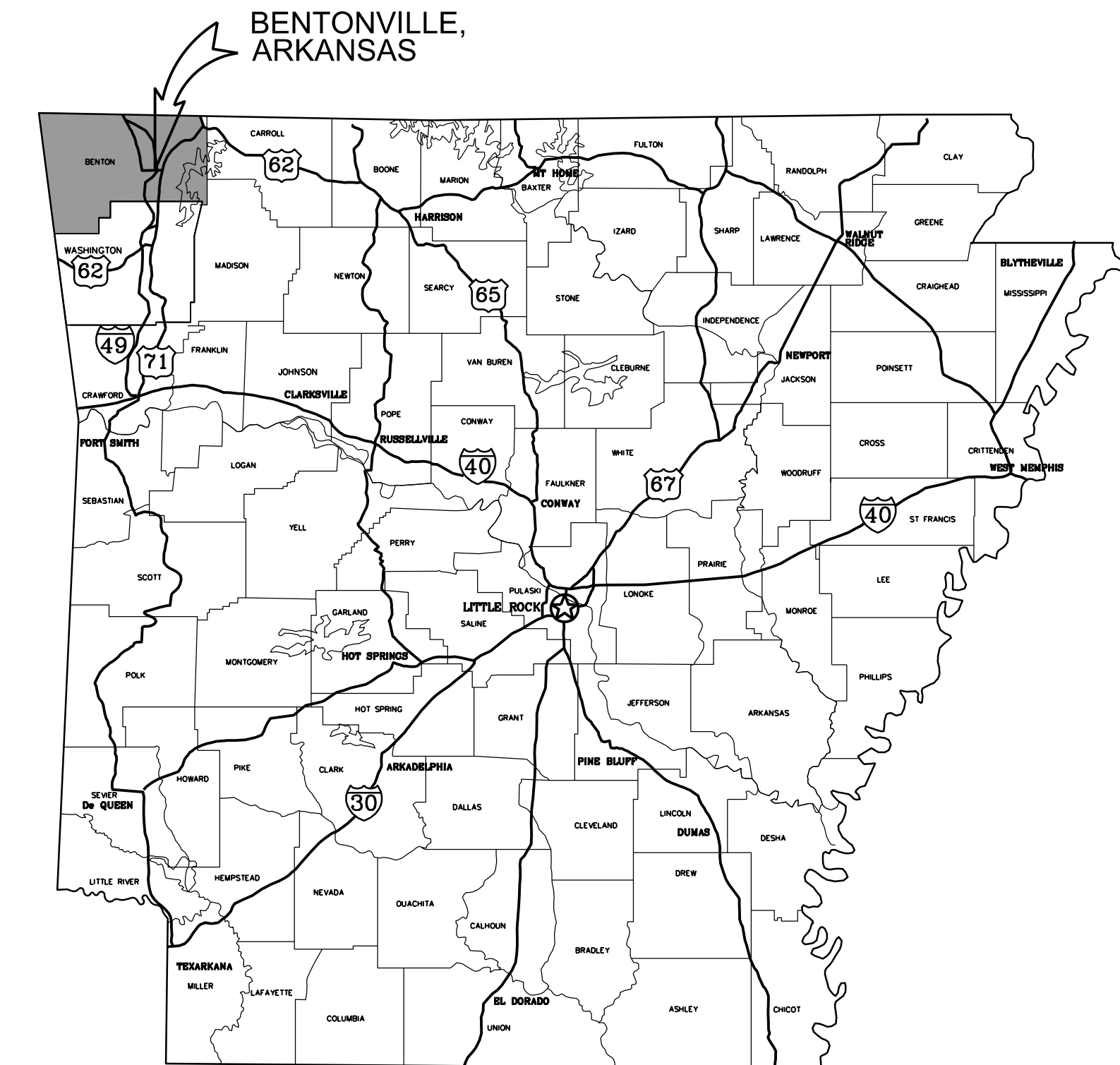
PRELIMINARY PLANS  
 FOR REVIEW ONLY  
 NOT FOR CONSTRUCTION



**LOCATION MAP**



**VICINITY MAP**  
 NO SCALE



**STATE MAP**

**GARVER PROJECT NO. 21T21080**  
**JANUARY 2022**

ARKANSAS ONE CALL



Know what's below.  
 Call before you dig.

SEE GENERAL NOTE 1, SHEET G-002 OF THESE PLANS.



4300 South J.B. Hunt Drive  
 Suite, 240  
 Rogers, AR 72758  
 (479) 257-9188



| REV. | DATE | DESCRIPTION | BY |
|------|------|-------------|----|
|      |      |             |    |
|      |      |             |    |
|      |      |             |    |



CITY OF BENTONVILLE  
 BENTONVILLE, ARKANSAS  
 WALTON BOULEVARD  
 AND MEDICAL CENTER  
 PARKWAY IMPROVEMENTS

COVER SHEET

JOB NO.: 21T21080  
 DATE: JAN. 2022  
 DESIGNED BY: ZTM  
 DRAWN BY: TWM

BAR IS ONE INCH ON ORIGINAL DRAWING  
 0 1" IF NOT ONE INCH ON THIS SHEET, ADJUST SCALES ACCORDINGLY.

DRAWING NUMBER  
**G-001**

**CONSTRUCTION GENERAL NOTES:**

- CAUTION: UNDERGROUND UTILITIES EXIST WITHIN AND ADJACENT TO THE LIMITS OF CONSTRUCTION. AN ATTEMPT HAS BEEN MADE TO LOCATE THESE UTILITIES ON THE PLANS; HOWEVER, ALL EXISTING UTILITIES MAY NOT BE SHOWN AND THE ACTUAL LOCATIONS OF THE UTILITIES MAY VARY FROM THE LOCATIONS SHOWN. SOME UTILITIES MAY HAVE BEEN RELOCATED SINCE THE TIME OF DESIGN AND THE CONTRACTOR'S NOTICE TO PROCEED. PRIOR TO BEGINNING ANY TYPE OF EXCAVATION, THE CONTRACTOR SHALL CONTACT THE UTILITIES INVOLVED AND MAKE ARRANGEMENTS FOR THE LOCATION OF THE UTILITIES ON THE GROUND. THE CONTRACTOR SHALL MAINTAIN THE UTILITY LOCATION MARKINGS UNTIL THEY ARE NO LONGER NECESSARY. ARKANSAS STATE LAW, THE UNDERGROUND FACILITIES DAMAGE PREVENTION ACT, REQUIRES TWO WORKING DAYS ADVANCE NOTIFICATION THROUGH THE ARKANSAS ONE-CALL SYSTEM CENTER BEFORE EXCAVATING USING MECHANIZED EQUIPMENT OR EXPLOSIVES (EXCEPT IN THE CASE OF EMERGENCY). THE ONE-CALL SYSTEM PHONE NUMBER IS 1-800-482-8998. THE CONTRACTOR IS ADVISED THAT THERE IS A SEVERE PENALTY FOR NOT MAKING THIS CALL. NOT ALL UTILITY COMPANIES ARE MEMBERS OF THE ARKANSAS ONE-CALL SYSTEM; THEREFORE, THE CONTRACTOR IS ADVISED TO CONTACT ALL NON-MEMBER UTILITIES AS WELL AS THE ONE-CALL SYSTEM. THE LOCATION OF THE EXISTING UTILITIES SHOWN IN THE PLANS ARE APPROXIMATE, AND ARE THE LOCATIONS AT THE TIME OF DESIGN.
- ALL PROPERTY CORNERS (IRON PINS) OR OTHER MONUMENTS LOCATED WITHIN THE PROPOSED CONSTRUCTION SHALL BE PROTECTED IN ACCORDANCE WITH SECTION 107.12 OF THE ARDOT STANDARD SPECIFICATIONS.
- ALL TREES THAT DO NOT DIRECTLY INTERFERE WITH THE PROPOSED CONSTRUCTION SHALL BE PROTECTED AND SAVED. CARE AND DISCRETION SHALL BE USED TO PROTECT TREES NOT IDENTIFIED FOR REMOVAL.
- THE TOPSOIL AND SOD QUANTITY INCLUDES THE EASEMENT ADJACENT TO THE ROAD. THE CONTRACTOR SHALL BE RESPONSIBLE TO GRADE TOPSOIL AND SOD ALL OTHER DISTURBED AREAS AT NO ADDITIONAL COMPENSATION.
- CONTRACTOR SHALL FIELD VERIFY LOCATIONS AND ELEVATIONS OF ALL UTILITIES, WATER LINES AND SANITARY SEWER LINES PRIOR TO CONSTRUCTION.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING U.S. MAILBOXES WITHIN THE PROJECT LIMITS IN SUCH A MANNER THAT THE PUBLIC MAY RECEIVE CONTINUED MAIL SERVICE. THE ENGINEER WILL DETERMINE WHICH MAILBOXES WILL BE REMOVED AND REPLACED.
- THE CONTRACTOR SHALL MOW GRASS WITHIN THE LIMITS OF THE PROJECT RIGHT OF WAY A MINIMUM OF TWO (2) TIMES AS DIRECTED BY THE OWNER AND THE ENGINEER. MOWING SHALL BE CONSIDERED SUBSIDIARY TO THE COST OF SEEDING AND SODDING.

**GOVERNING SPECIFICATIONS**

CITY OF BENTONVILLE, MINIMUM STANDARD SPECIFICATIONS FOR STREETS, LATEST EDITION, SUPPLEMENTED BY THE ARKANSAS STATE HIGHWAY COMMISSION STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION, EDITION OF 2014.

(THE CONTRACTOR IS RESPONSIBLE FOR OBTAINING THE ARDOT STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION, EDITION OF 2014.)

**DESIGN SPEED**

MEDICAL CENTER PARKWAY (SOUTH OF WALTON)= 25 MPH  
 MEDICAL CENTER PARKWAY (NORTH OF WALTON)= 30 MPH  
 WALTON BLVD.= 45 MPH

**LEGEND**

|     |       |     |                                 |  |                                  |
|-----|-------|-----|---------------------------------|--|----------------------------------|
| --- | TOS   | --- | EXISTING TOE-OF-SLOPE           |  | PROPOSED DECORATIVE GRAVEL       |
| --- | TOB   | --- | EXISTING TOP-OF-BANK            |  | DEMOLITION AREAS                 |
| --- | 1295  | --- | EXISTING MINOR CONTOUR          |  | PROPOSED CONCRETE                |
| --- | 1294  | --- | EXISTING MAJOR CONTOUR          |  | PROPOSED ASPHALT                 |
| --- | P L   | --- | EXISTING PROPERTY LINE          |  | EXISTING SIGN                    |
| --- |       | --- | EXISTING EASEMENT               |  | CONTROL POINT & BENCHMARK        |
| --- | G     | --- | EXISTING GAS UTILITY            |  | EXISTING TRANSFORMER             |
| --- | OHE   | --- | EXISTING ELECTRIC UTILITY       |  | EXISTING POWER POLE              |
| --- | FO    | --- | EXISTING FIBER OPTIC            |  | EXISTING LIGHT POLE              |
| --- | R / W | --- | EXISTING BACK OF CURB           |  | EXISTING TELECOMM BOX            |
| --- |       | --- | EXISTING RIGHT OF WAY           |  | EXISTING FIBER OPTIC PEDESTAL    |
| --- |       | --- | EXISTING EDGE OF PAVEMENT       |  | EXISTING GAS METER               |
| --- |       | --- | EXISTING GRAVEL DRIVE           |  | EXISTING WATER VALVE             |
| --- |       | --- | EXISTING FENCE                  |  | WATER METER                      |
| --- |       | --- | EXISTING SIDEWALK               |  | EXISTING FIRE HYDRANT            |
| --- | 8"SS  | --- | EXISTING STORM SEWER            |  | EXISTING STORM DRAIN INLET       |
| --- | 8"W   | --- | EXISTING SANITARY SEWER         |  | EXISTING SANITARY SEWER CLEANOUT |
| --- | FM    | --- | EXISTING WATER LINE             |  | EXISTING SANITARY SEWER MANHOLE  |
| --- | 12"SS | --- | PROPOSED FORCE MAIN             |  | EXISTING DECIDUOUS TREE          |
| --- | 12"W  | --- | PROPOSED SANITARY SEWER         |  | EXISTING CONIFEROUS TREE         |
| --- | R / W | --- | PROPOSED WATER LINE             |  | B-4 SOIL BORING LOCATION         |
| --- |       | --- | PROPOSED RIGHT OF WAY           |  |                                  |
| --- |       | --- | PROPOSED CENTERLINE             |  |                                  |
| --- |       | --- | PROPOSED BACK OF CURB           |  |                                  |
| --- |       | --- | PROPOSED SIDEWALK               |  |                                  |
| --- |       | --- | PROPOSED TOP-OF-BANK            |  |                                  |
| --- |       | --- | PROPOSED TOE-OF-SLOPE           |  |                                  |
| --- |       | --- | PROPOSED EASEMENT               |  |                                  |
| --- |       | --- | TEMPORARY CONSTRUCTION EASEMENT |  |                                  |
| --- |       | --- | PROPOSED FENCE                  |  |                                  |
| --- |       | --- | PROPOSED DITCH                  |  |                                  |
| --- |       | --- | PROPOSED MAILBOX                |  |                                  |
| --- |       | --- | PROPOSED LUMINAIRE POLE         |  |                                  |
| --- |       | --- | PROPOSED HANDICAP RAMP          |  |                                  |
| --- |       | --- | PROPOSED SIGN                   |  |                                  |
| --- |       | --- | PROPOSED DRAINAGE INLET         |  |                                  |

| SHEET INDEX    |  |
|----------------|--|
| G-001          | COVER SHEET  |
| G-002          | GENERAL NOTES, LEGEND, AND INDEX                         |
| C-101 TO C-105 | TYPICAL SECTIONS   |
| C-251 TO C-252 | DEMOLITION PLAN  |
| C-301 TO-302   | PAVEMENT MARKING AND SIGNAGE PLAN                        |
| C-501          | (S) MEDICAL CENTER PARKWAY PLAN AND PROFILE              |
| C-502          | (N) MEDICAL CENTER PARKWAY PLAN AND PROFILE              |
| C-503          | SE WALTON BOULEVARD PLAN AND PROFILE                     |
| C-521          | DRIVEWAY PROFILES  |
| C-551          | (S) MEDICAL CENTER PARKWAY DRAINAGE PLAN AND PROFILE     |
| C-552          | (N) MEDICAL CENTER PARKWAY DRAINAGE PLAN AND PROFILE     |
| C-553          | SE WALTON BOULEVARD DRAINAGE PLAN AND PROFILE            |
| C-600          | SIGNAL NOTES AND QUANTITIES                              |
| C-601 TO C-605 | SIGNALIZATION PLANS                                      |
| C-900          | WATERLINE AND SEWERLINE GENERAL NOTES                    |
| C-901          | (S) MEDICAL CENTER PARKWAY SEWER LINE A PLAN AND PROFILE |
| C-902          | (N) MEDICAL CENTER PARKWAY WATER AND SEWER ADJUSTMENTS   |
| X-101 TO X-106 | (S) MEDICAL CENTER PARKWAY CROSS SECTIONS                |
| X-201 TO X-204 | (N) MEDICAL CENTER PARKWAY CROSS SECTIONS                |
| X-301 TO X-304 | SE WALTON BOULEVARD CROSS SECTIONS                       |

| SURVEY CONTROL POINTS |             |             |           |                                     |
|-----------------------|-------------|-------------|-----------|-------------------------------------|
| POINT#                | NORTHING    | EASTING     | ELEVATION | DESCRIPTION                         |
| 1                     | 735792.2730 | 667153.9380 | 1262.837  | 1/2IN REBAR WITH GARVER CONTROL CAP |
| 2                     | 735787.1990 | 667344.3040 | 1263.215  | 1/2IN REBAR WITH GARVER CONTROL CAP |
| 3                     | 735520.4640 | 667143.3450 | 1268.987  | 1/2IN REBAR WITH GARVER CONTROL CAP |
| 4                     | 735900.4780 | 667048.6020 | 1264.443  | 1/2IN REBAR WITH GARVER CONTROL CAP |
| 5                     | 735897.3390 | 666850.9080 | 1269.965  | 1/2IN REBAR WITH GARVER CONTROL CAP |
| 6                     | 736330.1390 | 667092.9620 | 1263.765  | 1/2IN REBAR WITH GARVER CONTROL CAP |



© 2022 GARVER, LLC  
 THIS DOCUMENT, ALONG WITH THE IDEAS AND DESIGNS CONVEYED HEREIN, SHALL BE CONSIDERED INSTRUMENTS OF PROFESSIONAL SERVICE AND ARE PROPERTY OF GARVER, LLC. ANY USE, REPRODUCTION, OR DISTRIBUTION OF THIS DOCUMENT, ALONG WITH THE IDEAS AND DESIGN CONTAINED HEREIN, IS PROHIBITED UNLESS AUTHORIZED IN WRITING BY GARVER, LLC OR EXPLICITLY ALLOWED IN THE GOVERNING PROFESSIONAL SERVICE AGREEMENT FOR THIS WORK.

PRELIMINARY PLANS  
 FOR REVIEW ONLY  
 NOT FOR CONSTRUCTION

| REV. | DATE | DESCRIPTION | BY |
|------|------|-------------|----|
|      |      |             |    |



CITY OF BENTONVILLE  
 BENTONVILLE, ARKANSAS  
 WALTON BOULEVARD  
 AND MEDICAL CENTER  
 PARKWAY IMPROVEMENTS

GENERAL NOTES,  
 LEGEND, AND INDEX

JOB NO.: 21T21080  
 DATE: JAN. 2022  
 DESIGNED BY: ZTM  
 DRAWN BY: TWM

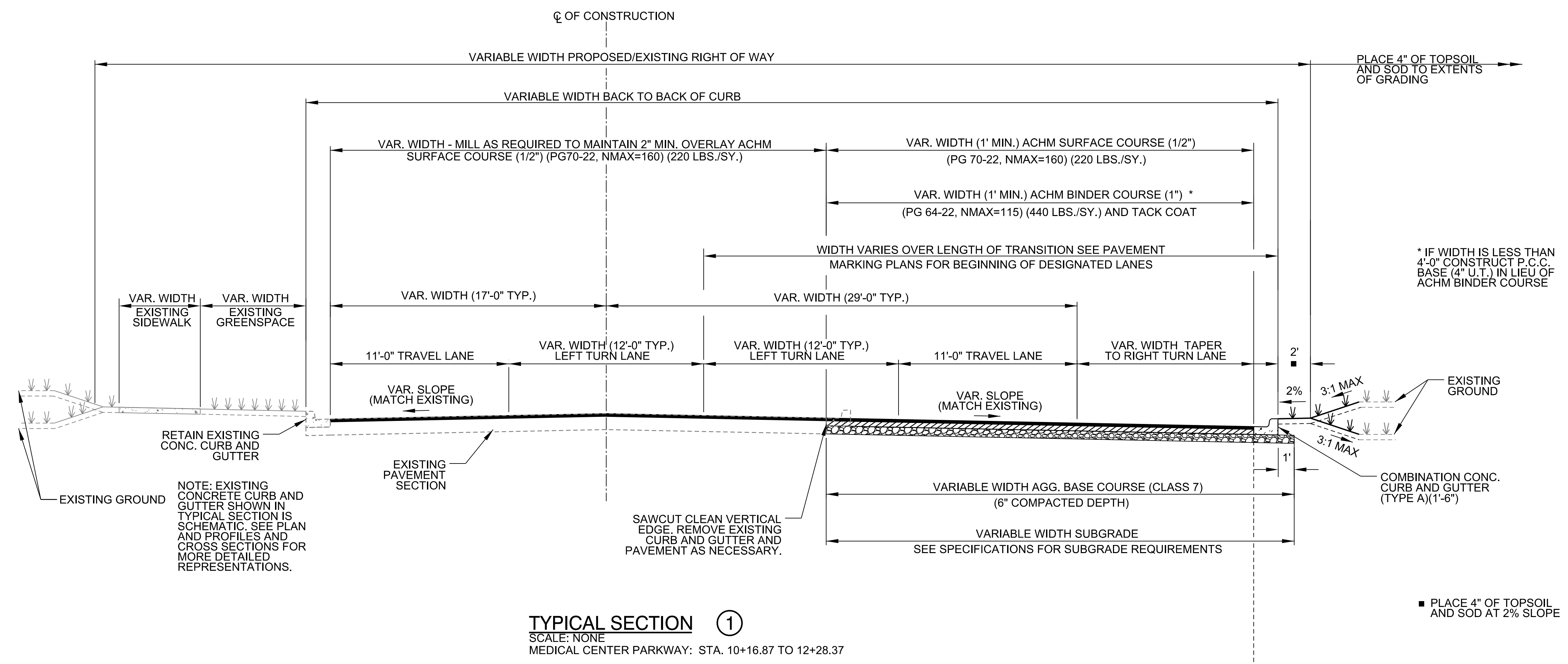
BAR IS ONE INCH ON ORIGINAL DRAWING  
 0 1" 1"  
 IF NOT ONE INCH ON THIS SHEET, ADJUST SCALES ACCORDINGLY.

DRAWING NUMBER  
**G-002**

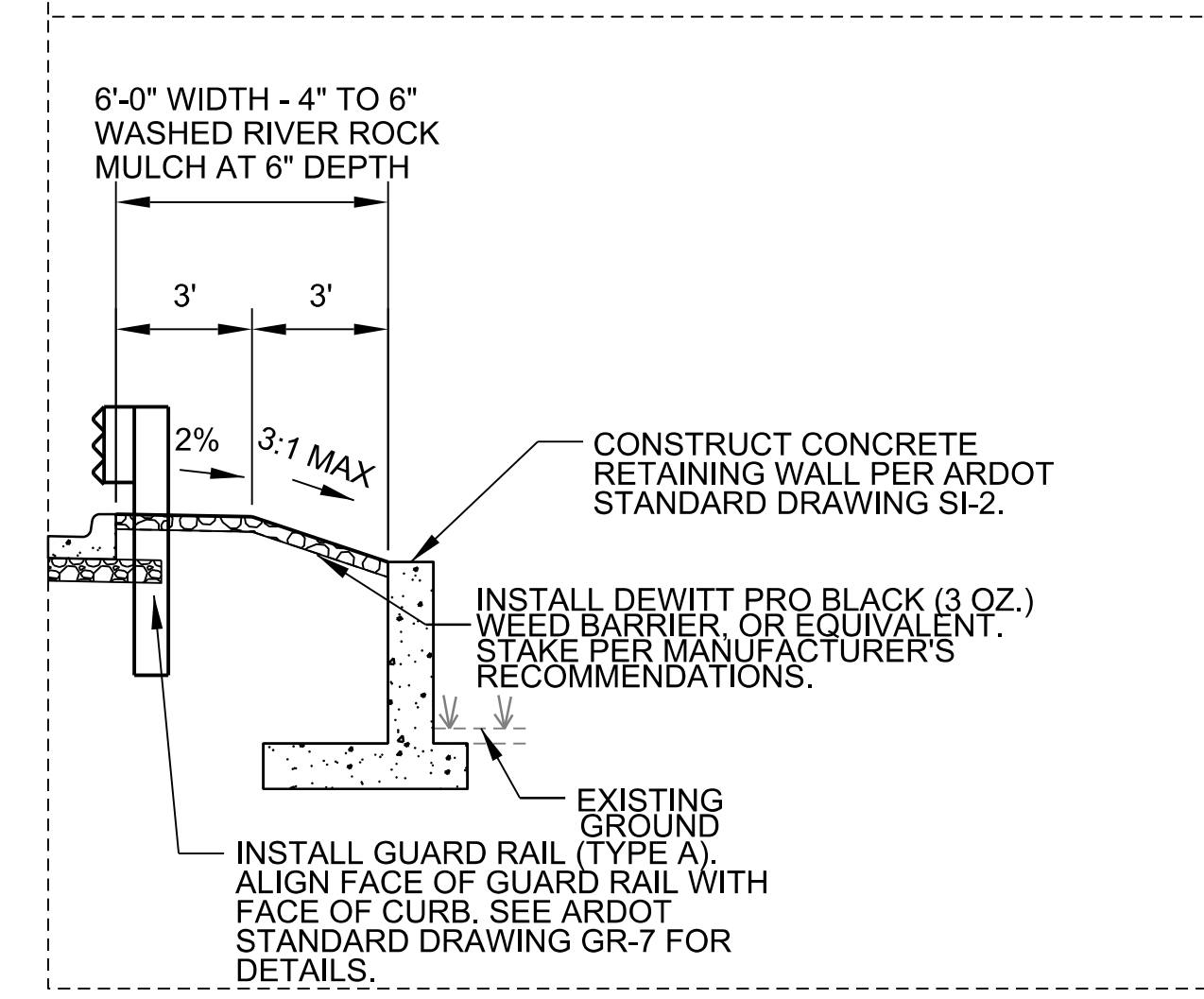


© 2022 GARVER, LLC  
 THIS DOCUMENT, ALONG WITH THE IDEAS AND DESIGNS CONVEYED HEREIN, SHALL BE CONSIDERED INSTRUMENTS OF PROFESSIONAL SERVICE AND ARE PROPERTY OF GARVER, LLC. ANY USE, REPRODUCTION, OR DISTRIBUTION OF THIS DOCUMENT, ALONG WITH THE IDEAS AND DESIGN CONTAINED HEREIN, IS PROHIBITED UNLESS AUTHORIZED IN WRITING BY GARVER, LLC OR EXPLICITLY ALLOWED IN THE GOVERNING PROFESSIONAL SERVICES AGREEMENT FOR THIS WORK.

PRELIMINARY PLANS  
 FOR REVIEW ONLY  
 NOT FOR CONSTRUCTION



**TYPICAL SECTION 1**  
 SCALE: NONE  
 MEDICAL CENTER PARKWAY: STA. 10+16.87 TO 12+28.37



**ALTERNATE SHOULDER SECTION**  
 MEDICAL CENTER PARKWAY: STA. 11+38.00 TO STA. 12+28.37 RT.

**TYPICAL SECTION GENERAL NOTES**

- REFER TO PLAN AND PROFILE SHEETS AND CROSS SECTIONS FOR DEVIATIONS FROM NORMAL SLOPES. NO CHANGES SHALL BE MADE FROM THE PLANNED SLOPES WITHOUT THE APPROVAL OF THE ENGINEER.
- THE FINAL 2 INCHES OF SURFACE COURSE IS TO BE PLACED AFTER ALL OTHER COURSES HAVE BEEN LAID. LONGITUDINAL JOINTS SHALL BE AT THE LANE LINES.
- PRIOR TO AND DURING PLACEMENT OF PAVEMENT IN FRONT OF THE CURB AND GUTTER, THE CONTRACTOR SHALL PROVIDE POSITIVE DRAINAGE AT ALL TIMES. THE METHODS USED SHALL BE APPROVED BY THE ENGINEER. PAYMENT FOR THIS WORK SHALL BE INCLUDED IN THE PRICE BID FOR THE VARIOUS CONTRACT ITEMS.
- THE THICKNESS OF AGG. BASE COURSE SHALL BE WITHIN PLUS OR MINUS 1/2" OF THE PLAN THICKNESS SHOWN. THE CONTRACTOR WILL CORRECT ANY DEFICIENT THICKNESS THAT DOES NOT MEET TOLERANCE INDICATED. PAYMENT WILL NOT BE MADE FOR MATERIAL PLACED IN EXCESS OF THE TOLERANCE INDICATED.
- THE EXISTING ASPHALT PAVEMENT TO BE REMOVED FROM THE REMAINING PAVEMENT SHALL BE SEPARATED BY SAWING ALONG A NEAT LINE. AFTER SAWING, THE PAVEMENT TO BE REMOVED SHALL BE CAREFULLY REMOVED IN A MANNER THAT WILL NOT DAMAGE THE PAVEMENT THAT IS TO REMAIN. ANY DAMAGE TO THE ASPHALT PAVEMENT THAT IS TO REMAIN IN PLACE SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE.
- IN-SITU SOILS MEETING THE REQUIREMENTS OUTLINED IN THE SPECIFICATIONS MAY BE UTILIZED AS SUBGRADE MATERIAL. NATURAL CHERTY IN-SITU SUBGRADE SOILS CLASSIFIED AS GC BY THE UNIFIED SOIL CLASSIFICATION SYSTEM WITH LESS THAN 40 PERCENT PASSING THE NO. 200 SIEVE AND A LIQUID LIMIT AND PLASTICITY INDEX OF MORE THAN OR EQUAL TO 40 AND 15, RESPECTIVELY, SHALL ONLY BE USED AS SUBGRADE MATERIAL WITH APPROVAL OF ENGINEER. IN-SITU SOILS ACCEPTABLE FOR USE AS SUBGRADE MATERIAL, OR MODIFIED TO BE SUITABLE, SHALL BE SCARIFIED TO A MINIMUM DEPTH OF 8 INCHES BELOW FINISH SUBGRADE AND RECOMPACTED TO CONFORM TO THE SPECIFICATIONS. SUBGRADE MATERIAL DETERMINED TO BE UNSUITABLE PER THE SPECIFICATIONS OR ENGINEER SHALL BE UNDERCUT TO A MINIMUM DEPTH OF 24" UNLESS APPROVED OTHERWISE BY ENGINEER.
- TRANSVERSE EXPANSION JOINTS SHALL BE PLACED IN CONCRETE WALKS AT AN INTERVAL EQUAL TO FIVE (5) TIMES THE SIDEWALK WIDTH. CONTRACTION JOINTS SHALL BE PLACED AT AN INTERVAL MATCHING THE SIDEWALK WIDTH.

| REV. | DATE | DESCRIPTION | BY |
|------|------|-------------|----|
|      |      |             |    |
|      |      |             |    |
|      |      |             |    |



CITY OF BENTONVILLE  
 BENTONVILLE, ARKANSAS  
 WALTON BOULEVARD  
 AND MEDICAL CENTER  
 PARKWAY IMPROVEMENTS

TYPICAL SECTIONS

JOB NO.: 21T21080  
 DATE: JAN. 2022  
 DESIGNED BY: ZTM  
 DRAWN BY: TWM

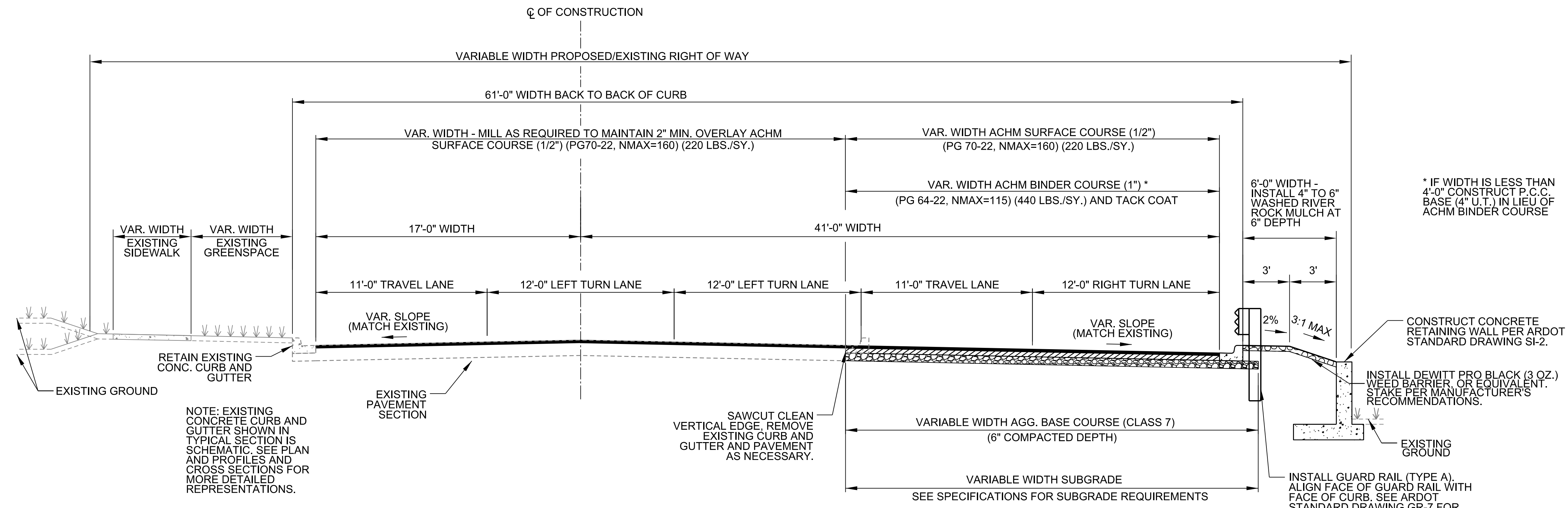
BAR IS ONE INCH ON ORIGINAL DRAWING  
 0 1" ON THIS SHEET  
 IF NOT ONE INCH ON THIS SHEET, ADJUST SCALES ACCORDINGLY.

DRAWING NUMBER  
**C-101**

TSAUSTIN 1/26/2022 4:06:55 PM  
 WORKSPACE: Garver\_2012  
 \\garver\cadd\projects\2021\21T21080 - Bentonville Walton Blvd-SE Med Ctr Imps\Drawings\WMC\_C101\_TS.dgn  
 REVISION DATE:



© 2022 GARVER, LLC  
 THIS DOCUMENT, ALONG WITH THE IDEAS AND DESIGNS CONVEYED HEREIN, SHALL BE CONSIDERED INSTRUMENTS OF PROFESSIONAL SERVICE AND ARE PROPERTY OF GARVER, LLC. ANY USE, REPRODUCTION, OR DISTRIBUTION OF THIS DOCUMENT, ALONG WITH THE IDEAS AND DESIGN CONTAINED HEREIN, IS PROHIBITED UNLESS AUTHORIZED IN WRITING BY GARVER, LLC OR EXPLICITLY ALLOWED IN THE GOVERNING PROFESSIONAL SERVICES AGREEMENT FOR THIS WORK.



**TYPICAL SECTION ②**  
 SCALE: NONE  
 MEDICAL CENTER PARKWAY: STA. 12+28.37 TO 13+28.24

\* IF WIDTH IS LESS THAN 4'-0" CONSTRUCT P.C.C. BASE (4" U.T.) IN LIEU OF ACHM BINDER COURSE

NOTE: EXISTING CONCRETE CURB AND GUTTER SHOWN IN TYPICAL SECTION IS SCHEMATIC. SEE PLAN AND PROFILES AND CROSS SECTIONS FOR MORE DETAILED REPRESENTATIONS.

SAWCUT CLEAN VERTICAL EDGE, REMOVE EXISTING CURB AND GUTTER AND PAVEMENT AS NECESSARY.

INSTALL DEWITT PRO BLACK (3 OZ.) WEED BARRIER, OR EQUIVALENT. STAKE PER MANUFACTURER'S RECOMMENDATIONS.

INSTALL GUARD RAIL (TYPE A), ALIGN FACE OF GUARD RAIL WITH FACE OF CURB. SEE ARDOT STANDARD DRAWING GR-7 FOR DETAILS.

**TYPICAL SECTION GENERAL NOTES**

- REFER TO PLAN AND PROFILE SHEETS AND CROSS SECTIONS FOR DEVIATIONS FROM NORMAL SLOPES. NO CHANGES SHALL BE MADE FROM THE PLANNED SLOPES WITHOUT THE APPROVAL OF THE ENGINEER.
- THE FINAL 2 INCHES OF SURFACE COURSE IS TO BE PLACED AFTER ALL OTHER COURSES HAVE BEEN LAID. LONGITUDINAL JOINTS SHALL BE AT THE LANE LINES.
- PRIOR TO AND DURING PLACEMENT OF PAVEMENT IN FRONT OF THE CURB AND GUTTER, THE CONTRACTOR SHALL PROVIDE POSITIVE DRAINAGE AT ALL TIMES. THE METHODS USED SHALL BE APPROVED BY THE ENGINEER. PAYMENT FOR THIS WORK SHALL BE CONSIDERED INCLUDED IN THE PRICE BID FOR THE VARIOUS CONTRACT ITEMS.
- THE THICKNESS OF AGG. BASE COURSE SHALL BE WITHIN PLUS OR MINUS 1/2" OF THE PLAN THICKNESS SHOWN. THE CONTRACTOR WILL CORRECT ANY DEFICIENT THICKNESS THAT DOES NOT MEET TOLERANCE INDICATED. PAYMENT WILL NOT BE MADE FOR MATERIAL PLACED IN EXCESS OF THE TOLERANCE INDICATED.
- THE EXISTING ASPHALT PAVEMENT TO BE REMOVED FROM THE REMAINING PAVEMENT SHALL BE SEPARATED BY SAWING ALONG A NEAT LINE. AFTER SAWING, THE PAVEMENT TO BE REMOVED SHALL BE CAREFULLY REMOVED IN A MANNER THAT WILL NOT DAMAGE THE PAVEMENT THAT IS TO REMAIN. ANY DAMAGE TO THE ASPHALT PAVEMENT THAT IS TO REMAIN IN PLACE SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE.
- IN-SITU SOILS MEETING THE REQUIREMENTS OUTLINED IN THE SPECIFICATIONS MAY BE UTILIZED AS SUBGRADE MATERIAL. NATURAL CHERTY IN-SITU SUBGRADE SOILS CLASSIFIED AS GC BY THE UNIFIED SOIL CLASSIFICATION SYSTEM WITH LESS THAN 40 PERCENT PASSING THE NO. 200 SIEVE AND A LIQUID LIMIT AND PLASTICITY INDEX OF MORE THAN OR EQUAL TO 40 AND 15, RESPECTIVELY, SHALL ONLY BE USED AS SUBGRADE MATERIAL WITH APPROVAL OF ENGINEER. IN-SITU SOILS ACCEPTABLE FOR USE AS SUBGRADE MATERIAL, OR MODIFIED TO BE SUITABLE, SHALL BE SCARIFIED TO A MINIMUM DEPTH OF 8 INCHES BELOW FINISH SUBGRADE AND RECOMPACTED TO CONFORM TO THE SPECIFICATIONS. SUBGRADE MATERIAL DETERMINED TO BE UNSUITABLE PER THE SPECIFICATIONS OR ENGINEER SHALL BE UNDERCUT TO A MINIMUM DEPTH OF 24" UNLESS APPROVED OTHERWISE BY ENGINEER.
- TRANSVERSE EXPANSION JOINTS SHALL BE PLACED IN CONCRETE WALKS AT AN INTERVAL EQUAL TO FIVE (5) TIMES THE SIDEWALK WIDTH. CONTRACTION JOINTS SHALL BE PLACED AT AN INTERVAL MATCHING THE SIDEWALK WIDTH.

**PRELIMINARY PLANS  
 FOR REVIEW ONLY  
 NOT FOR CONSTRUCTION**

| REV. | DATE | DESCRIPTION | BY |
|------|------|-------------|----|
|      |      |             |    |
|      |      |             |    |
|      |      |             |    |



**CITY OF BENTONVILLE**  
 BENTONVILLE, ARKANSAS  
**WALTON BOULEVARD  
 AND MEDICAL CENTER  
 PARKWAY IMPROVEMENTS**

**TYPICAL SECTIONS**

JOB NO.: 21T21080  
 DATE: JAN. 2022  
 DESIGNED BY: ZTM  
 DRAWN BY: TWM

BAR IS ONE INCH ON ORIGINAL DRAWING  
 0 1"  
 IF NOT ONE INCH ON THIS SHEET, ADJUST SCALES ACCORDINGLY.

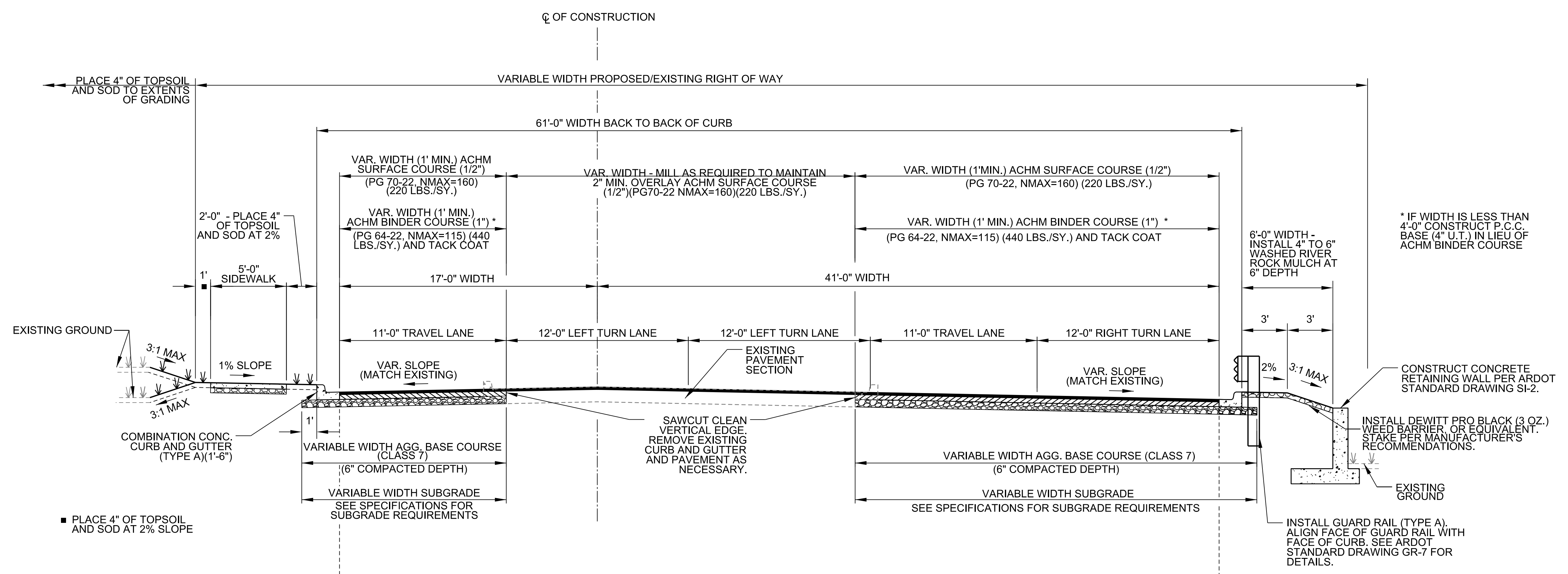
**DRAWING NUMBER  
 C-102**

TSAUSTIN 1/26/2022 4:06:56 PM  
 WORKSPACE: Garver\_2012  
 \\garver\cadd\projects\2021\21T21080 - Bentonville Walton Blvd-SE Med Ctr Imps\Drawings\WMC\_C101\_TS.dgn  
 REVISION DATE:

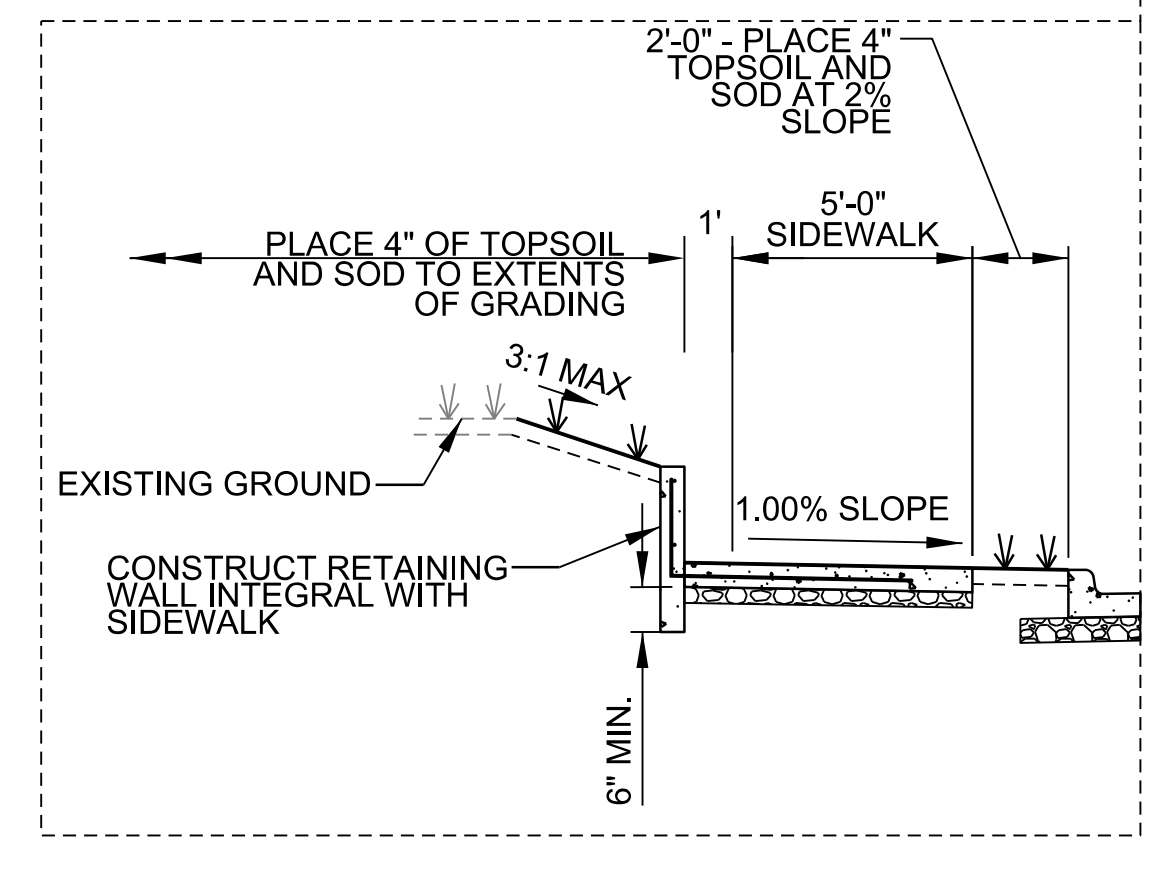


© 2022 GARVER, LLC  
 THIS DOCUMENT, ALONG WITH THE IDEAS AND DESIGNS CONVEYED HEREIN, SHALL BE CONSIDERED INSTRUMENTS OF PROFESSIONAL SERVICE AND ARE PROPERTY OF GARVER, LLC. ANY USE, REPRODUCTION, OR DISTRIBUTION OF THIS DOCUMENT, ALONG WITH THE IDEAS AND DESIGNS CONTAINED HEREIN, IS PROHIBITED UNLESS AUTHORIZED IN WRITING BY GARVER, LLC OR EXPLICITLY ALLOWED IN THE GOVERNING PROFESSIONAL SERVICES AGREEMENT FOR THIS WORK.

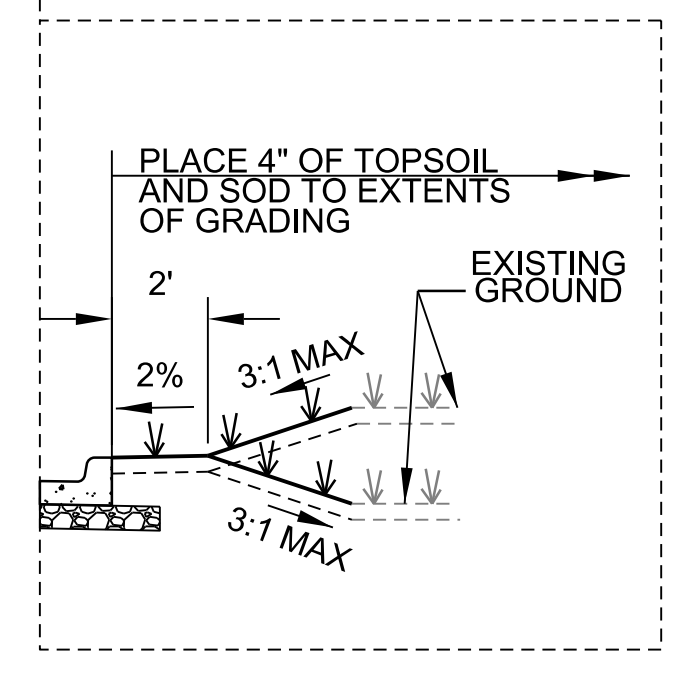
PRELIMINARY PLANS  
 FOR REVIEW ONLY  
 NOT FOR CONSTRUCTION



**TYPICAL SECTION 3**  
 SCALE: NONE  
 MEDICAL CENTER PARKWAY: STA. 13+28.24 TO 14+71.89



**ALTERNATE INTEGRAL WALL SECTION**  
 MEDICAL CENTER PARKWAY: STA. 13+75.00 TO STA. 14+19.25 LT.



**ALTERNATE SHOULDER SECTION**  
 MEDICAL CENTER PARKWAY: STA. 13+48.00 TO STA. 14+71.89 LT.

NOTE: EXISTING CONCRETE CURB AND GUTTER LOCATION SHOWN IN TYPICAL SECTION IS SCHEMATIC. SEE PLAN AND PROFILES AND CROSS SECTIONS FOR MORE DETAILED REPRESENTATIONS.

**TYPICAL SECTION GENERAL NOTES**

- REFER TO PLAN AND PROFILE SHEETS AND CROSS SECTIONS FOR DEVIATIONS FROM NORMAL SLOPES. NO CHANGES SHALL BE MADE FROM THE PLANNED SLOPES WITHOUT THE APPROVAL OF THE ENGINEER.
- THE FINAL 2 INCHES OF SURFACE COURSE IS TO BE PLACED AFTER ALL OTHER COURSES HAVE BEEN LAID. LONGITUDINAL JOINTS SHALL BE AT THE LANE LINES.
- PRIOR TO AND DURING PLACEMENT OF PAVEMENT IN FRONT OF THE CURB AND GUTTER, THE CONTRACTOR SHALL PROVIDE POSITIVE DRAINAGE AT ALL TIMES. THE METHODS USED SHALL BE APPROVED BY THE ENGINEER. PAYMENT FOR THIS WORK SHALL BE CONSIDERED INCLUDED IN THE PRICE BID FOR THE VARIOUS CONTRACT ITEMS.
- THE THICKNESS OF AGG. BASE COURSE SHALL BE WITHIN PLUS OR MINUS 1/2" OF THE PLAN THICKNESS SHOWN. THE CONTRACTOR WILL CORRECT ANY DEFICIENT THICKNESS THAT DOES NOT MEET TOLERANCE INDICATED. PAYMENT WILL NOT BE MADE FOR MATERIAL PLACED IN EXCESS OF THE TOLERANCE INDICATED.
- THE EXISTING ASPHALT PAVEMENT TO BE REMOVED FROM THE REMAINING PAVEMENT SHALL BE SEPARATED BY SAWING ALONG A NEAT LINE. AFTER SAWING, THE PAVEMENT TO BE REMOVED SHALL BE CAREFULLY REMOVED IN A MANNER THAT WILL NOT DAMAGE THE PAVEMENT THAT IS TO REMAIN. ANY DAMAGE TO THE ASPHALT PAVEMENT THAT IS TO REMAIN IN PLACE SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE.
- IN-SITU SOILS MEETING THE REQUIREMENTS OUTLINED IN THE SPECIFICATIONS MAY BE UTILIZED AS SUBGRADE MATERIAL. NATURAL CHERTY IN-SITU SUBGRADE SOILS CLASSIFIED AS GC BY THE UNIFIED SOIL CLASSIFICATION SYSTEM WITH LESS THAN 40 PERCENT PASSING THE NO. 200 SIEVE AND A LIQUID LIMIT AND PLASTICITY INDEX OF MORE THAN OR EQUAL TO 40 AND 15, RESPECTIVELY, SHALL ONLY BE USED AS SUBGRADE MATERIAL WITH APPROVAL OF ENGINEER. IN-SITU SOILS ACCEPTABLE FOR USE AS SUBGRADE MATERIAL, OR MODIFIED TO BE SUITABLE, SHALL BE SCARIFIED TO A MINIMUM DEPTH OF 8 INCHES BELOW FINISH SUBGRADE AND RECOMPACTED TO CONFORM TO THE SPECIFICATIONS. SUBGRADE MATERIAL DETERMINED TO BE UNSUITABLE PER THE SPECIFICATIONS OR ENGINEER SHALL BE UNDERCUT TO A MINIMUM DEPTH OF 24" UNLESS APPROVED OTHERWISE BY ENGINEER.
- TRANSVERSE EXPANSION JOINTS SHALL BE PLACED IN CONCRETE WALKS AT AN INTERVAL EQUAL TO FIVE (5) TIMES THE SIDEWALK WIDTH. CONTRACTION JOINTS SHALL BE PLACED AT AN INTERVAL MATCHING THE SIDEWALK WIDTH.

| BY | DESCRIPTION | DATE | REV. |
|----|-------------|------|------|
|    |             |      |      |
|    |             |      |      |
|    |             |      |      |



CITY OF BENTONVILLE  
 BENTONVILLE, ARKANSAS  
 WALTON BOULEVARD  
 AND MEDICAL CENTER  
 PARKWAY IMPROVEMENTS

**TYPICAL SECTIONS**

JOB NO.: 21T21080  
 DATE: JAN. 2022  
 DESIGNED BY: ZTM  
 DRAWN BY: TWM

BAR IS ONE INCH ON ORIGINAL DRAWING  
 0 1"  
 IF NOT ONE INCH ON THIS SHEET, ADJUST SCALES ACCORDINGLY.

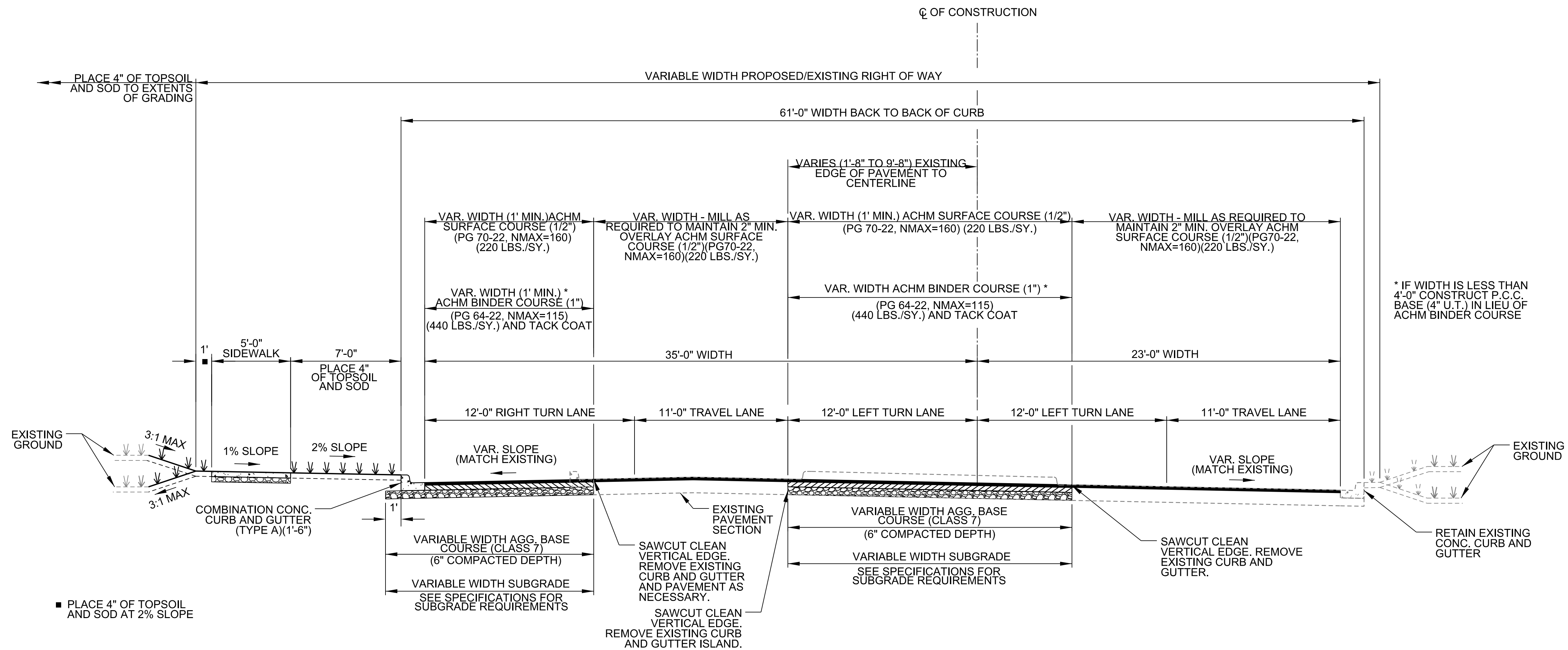
DRAWING NUMBER  
**C-103**

TSAUSTIN 1/26/2022 4:06:56 PM  
 WORKSPACE: Garver\_2012  
 \\garver\projects\2021\21T21080 - Bentonville Walton Blvd-SE Med Ctr Imps\Drawings\WMC\_C101\_TS.dgn  
 REVISION DATE:



© 2022 GARVER, LLC  
 THIS DOCUMENT, ALONG WITH THE IDEAS AND DESIGNS CONVEYED HEREIN, SHALL BE CONSIDERED INSTRUMENTS OF PROFESSIONAL SERVICE AND ARE PROPERTY OF GARVER, LLC. ANY USE, REPRODUCTION, OR DISTRIBUTION OF THIS DOCUMENT, ALONG WITH THE IDEAS AND DESIGN CONTAINED HEREIN, IS PROHIBITED UNLESS AUTHORIZED IN WRITING BY GARVER, LLC OR EXPLICITLY ALLOWED IN THE GOVERNING PROFESSIONAL SERVICES AGREEMENT FOR THIS WORK.

PRELIMINARY PLANS  
 FOR REVIEW ONLY  
 NOT FOR CONSTRUCTION



**TYPICAL SECTION ④**  
 SCALE: NONE  
 MEDICAL CENTER PARKWAY: STA. 20+27.90 TO 22+69.40  
 STA. 22+69.40 TO 23+05.36 TRANSITION TO TYPICAL SECTION ⑤

NOTE: EXISTING CONCRETE CURB AND GUTTER LOCATION SHOWN IN TYPICAL SECTION IS SCHEMATIC. SEE PLAN AND PROFILES AND CROSS SECTIONS FOR MORE DETAILED REPRESENTATIONS.

**TYPICAL SECTION GENERAL NOTES**

- REFER TO PLAN AND PROFILE SHEETS AND CROSS SECTIONS FOR DEVIATIONS FROM NORMAL SLOPES. NO CHANGES SHALL BE MADE FROM THE PLANNED SLOPES WITHOUT THE APPROVAL OF THE ENGINEER.
- THE FINAL 2 INCHES OF SURFACE COURSE IS TO BE PLACED AFTER ALL OTHER COURSES HAVE BEEN LAID. LONGITUDINAL JOINTS SHALL BE AT THE LANE LINES.
- PRIOR TO AND DURING PLACEMENT OF PAVEMENT IN FRONT OF THE CURB AND GUTTER, THE CONTRACTOR SHALL PROVIDE POSITIVE DRAINAGE AT ALL TIMES. THE METHODS USED SHALL BE APPROVED BY THE ENGINEER. PAYMENT FOR THIS WORK SHALL BE CONSIDERED INCLUDED IN THE PRICE BID FOR THE VARIOUS CONTRACT ITEMS.
- THE THICKNESS OF AGG. BASE COURSE SHALL BE WITHIN PLUS OR MINUS 1/2" OF THE PLAN THICKNESS SHOWN. THE CONTRACTOR WILL CORRECT ANY DEFICIENT THICKNESS THAT DOES NOT MEET TOLERANCE INDICATED. PAYMENT WILL NOT BE MADE FOR MATERIAL PLACED IN EXCESS OF THE TOLERANCE INDICATED.
- THE EXISTING ASPHALT PAVEMENT TO BE REMOVED FROM THE REMAINING PAVEMENT SHALL BE SEPARATED BY SAWING ALONG A NEAT LINE. AFTER SAWING, THE PAVEMENT TO BE REMOVED SHALL BE CAREFULLY REMOVED IN A MANNER THAT WILL NOT DAMAGE THE PAVEMENT THAT IS TO REMAIN. ANY DAMAGE TO THE ASPHALT PAVEMENT THAT IS TO REMAIN IN PLACE SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE.
- IN-SITU SOILS MEETING THE REQUIREMENTS OUTLINED IN THE SPECIFICATIONS MAY BE UTILIZED AS SUBGRADE MATERIAL. NATURAL CHERTY IN-SITU SUBGRADE SOILS CLASSIFIED AS GC BY THE UNIFIED SOIL CLASSIFICATION SYSTEM WITH LESS THAN 40 PERCENT PASSING THE NO. 200 SIEVE AND A LIQUID LIMIT AND PLASTICITY INDEX OF MORE THAN OR EQUAL TO 40 AND 15, RESPECTIVELY, SHALL ONLY BE USED AS SUBGRADE MATERIAL WITH APPROVAL OF ENGINEER. IN-SITU SOILS ACCEPTABLE FOR USE AS SUBGRADE MATERIAL, OR MODIFIED TO BE SUITABLE, SHALL BE SCARIFIED TO A MINIMUM DEPTH OF 8 INCHES BELOW FINISH SUBGRADE AND RECOMPACTED TO CONFORM TO THE SPECIFICATIONS. SUBGRADE MATERIAL DETERMINED TO BE UNSUITABLE PER THE SPECIFICATIONS OR ENGINEER SHALL BE UNDERCUT TO A MINIMUM DEPTH OF 24" UNLESS APPROVED OTHERWISE BY ENGINEER.
- TRANSVERSE EXPANSION JOINTS SHALL BE PLACED IN CONCRETE WALKS AT AN INTERVAL EQUAL TO FIVE (5) TIMES THE SIDEWALK WIDTH. CONTRACTION JOINTS SHALL BE PLACED AT AN INTERVAL MATCHING THE SIDEWALK WIDTH.

| BY | DESCRIPTION | DATE | REV. |
|----|-------------|------|------|
|    |             |      |      |
|    |             |      |      |
|    |             |      |      |



CITY OF BENTONVILLE  
 BENTONVILLE, ARKANSAS  
 WALTON BOULEVARD  
 AND MEDICAL CENTER  
 PARKWAY IMPROVEMENTS

TYPICAL SECTIONS

JOB NO.: 21T21080  
 DATE: JAN. 2022  
 DESIGNED BY: ZTM  
 DRAWN BY: TWM

BAR IS ONE INCH ON ORIGINAL DRAWING  
 0 1" IF NOT ONE INCH ON THIS SHEET, ADJUST SCALES ACCORDINGLY.

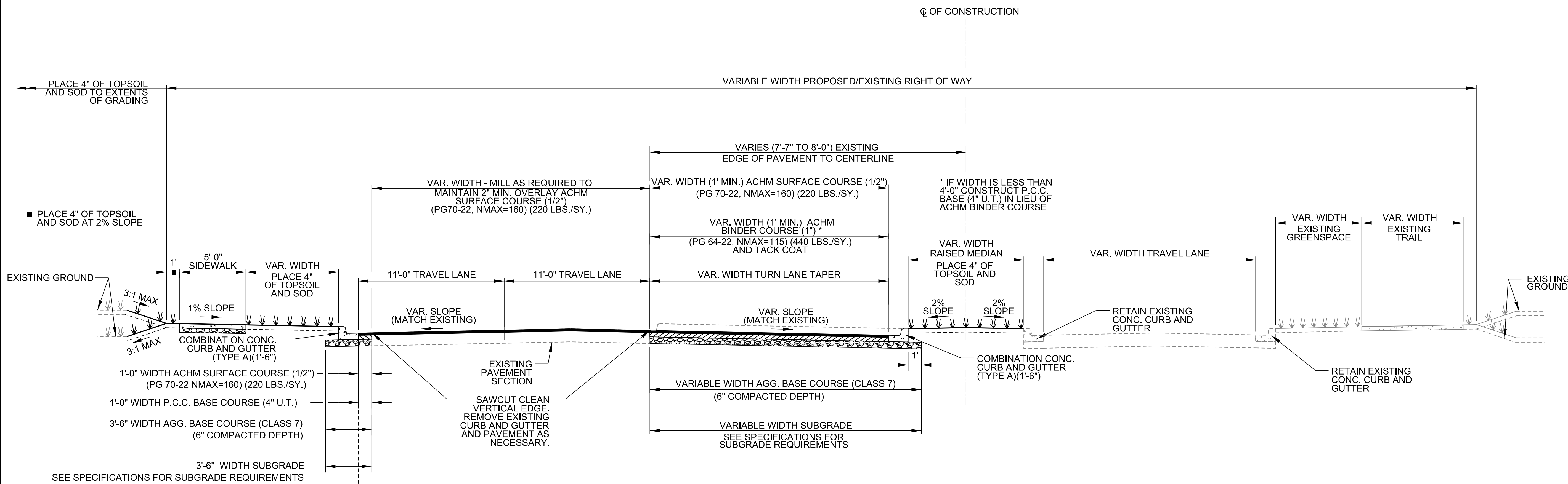
DRAWING NUMBER  
**C-104**

TSAUSTIN 1/26/2022 4:06:57 PM  
 WORKSPACE: Garver\_2012  
 \\garver\cadd\project\2021\21T21080 - Bentonville Walton Blvd-SE Med Ctr Imps\Drawings\WMC\_C101\_TS.dgn  
 REVISION DATE:



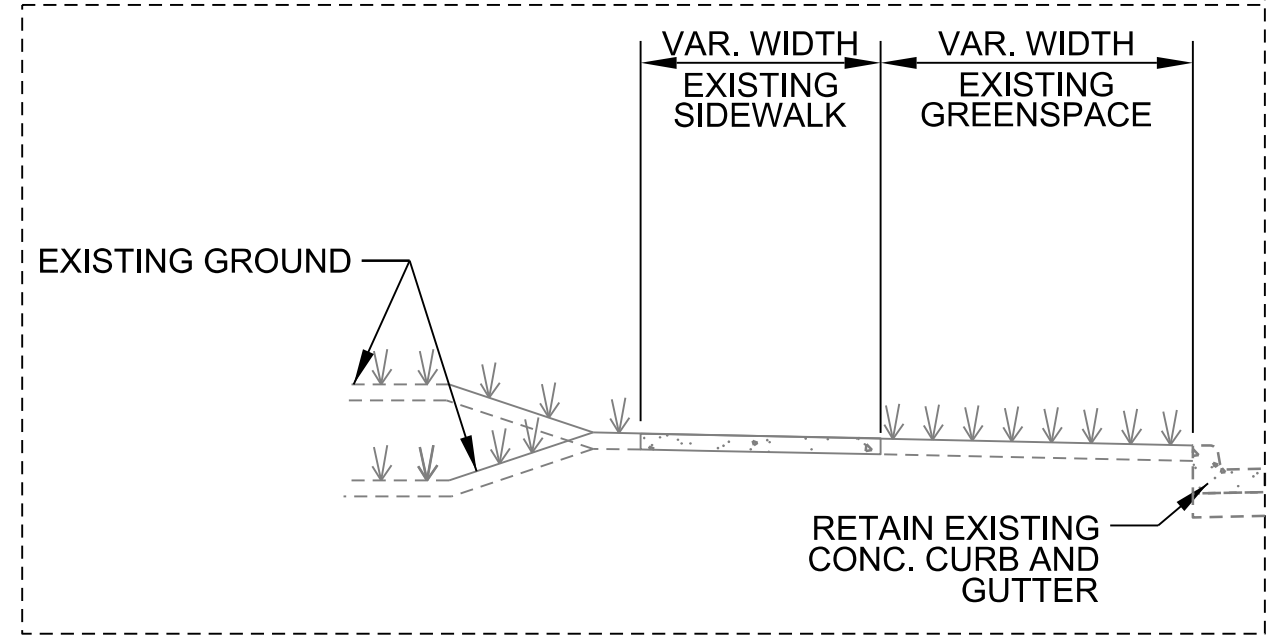
© 2022 GARVER, LLC  
 THIS DOCUMENT, ALONG WITH THE IDEAS AND DESIGNS CONVEYED HEREIN, SHALL BE CONSIDERED INSTRUMENTS OF PROFESSIONAL SERVICE AND ARE PROPERTY OF GARVER, LLC. ANY USE, REPRODUCTION, OR DISTRIBUTION OF THIS DOCUMENT, ALONG WITH THE IDEAS AND DESIGN CONTAINED HEREIN, IS PROHIBITED UNLESS AUTHORIZED IN WRITING BY GARVER, LLC OR EXPLICITLY ALLOWED IN THE GOVERNING PROFESSIONAL SERVICES AGREEMENT FOR THIS WORK.

PRELIMINARY PLANS  
 FOR REVIEW ONLY  
 NOT FOR CONSTRUCTION



**TYPICAL SECTION ⑤**  
 SCALE: NONE  
 MEDICAL CENTER PARKWAY: STA. 23+05.36 TO 23+52.50

NOTE: EXISTING CONCRETE CURB AND GUTTER LOCATION SHOWN IN TYPICAL SECTION IS SCHEMATIC. SEE PLAN AND PROFILES AND CROSS SECTIONS FOR MORE DETAILED REPRESENTATIONS.



**ALTERNATE CURB SECTION**  
 MEDICAL CENTER PARKWAY: STA. 23+25.00 TO STA. 23+52.50 LT.

**TYPICAL SECTION GENERAL NOTES**

- REFER TO PLAN AND PROFILE SHEETS AND CROSS SECTIONS FOR DEVIATIONS FROM NORMAL SLOPES. NO CHANGES SHALL BE MADE FROM THE PLANNED SLOPES WITHOUT THE APPROVAL OF THE ENGINEER.
- THE FINAL 2 INCHES OF SURFACE COURSE IS TO BE PLACED AFTER ALL OTHER COURSES HAVE BEEN LAID. LONGITUDINAL JOINTS SHALL BE AT THE LANE LINES.
- PRIOR TO AND DURING PLACEMENT OF PAVEMENT IN FRONT OF THE CURB AND GUTTER, THE CONTRACTOR SHALL PROVIDE POSITIVE DRAINAGE AT ALL TIMES. THE METHODS USED SHALL BE APPROVED BY THE ENGINEER. PAYMENT FOR THIS WORK SHALL BE CONSIDERED INCLUDED IN THE PRICE BID FOR THE VARIOUS CONTRACT ITEMS.
- THE THICKNESS OF AGG. BASE COURSE SHALL BE WITHIN PLUS OR MINUS 1/2" OF THE PLAN THICKNESS SHOWN. THE CONTRACTOR WILL CORRECT ANY DEFICIENT THICKNESS THAT DOES NOT MEET TOLERANCE INDICATED. PAYMENT WILL NOT BE MADE FOR MATERIAL PLACED IN EXCESS OF THE TOLERANCE INDICATED.
- THE EXISTING ASPHALT PAVEMENT TO BE REMOVED FROM THE REMAINING PAVEMENT SHALL BE SEPARATED BY SAWING ALONG A NEAT LINE. AFTER SAWING, THE PAVEMENT TO BE REMOVED SHALL BE CAREFULLY REMOVED IN A MANNER THAT WILL NOT DAMAGE THE PAVEMENT THAT IS TO REMAIN. ANY DAMAGE TO THE ASPHALT PAVEMENT THAT IS TO REMAIN IN PLACE SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE.
- IN-SITU SOILS MEETING THE REQUIREMENTS OUTLINED IN THE SPECIFICATIONS MAY BE UTILIZED AS SUBGRADE MATERIAL. NATURAL CHERTY IN-SITU SUBGRADE SOILS CLASSIFIED AS GC BY THE UNIFIED SOIL CLASSIFICATION SYSTEM WITH LESS THAN 40 PERCENT PASSING THE NO. 200 SIEVE AND A LIQUID LIMIT AND PLASTICITY INDEX OF MORE THAN OR EQUAL TO 40 AND 15, RESPECTIVELY, SHALL ONLY BE USED AS SUBGRADE MATERIAL WITH APPROVAL OF ENGINEER. IN-SITU SOILS ACCEPTABLE FOR USE AS SUBGRADE MATERIAL, OR MODIFIED TO BE SUITABLE, SHALL BE SCARIFIED TO A MINIMUM DEPTH OF 8 INCHES BELOW FINISH SUBGRADE AND RECOMPACTED TO CONFORM TO THE SPECIFICATIONS. SUBGRADE MATERIAL DETERMINED TO BE UNSUITABLE PER THE SPECIFICATIONS OR ENGINEER SHALL BE UNDERCUT TO A MINIMUM DEPTH OF 24" UNLESS APPROVED OTHERWISE BY ENGINEER.
- TRANSVERSE EXPANSION JOINTS SHALL BE PLACED IN CONCRETE WALKS AT AN INTERVAL EQUAL TO FIVE (5) TIMES THE SIDEWALK WIDTH. CONTRACTION JOINTS SHALL BE PLACED AT AN INTERVAL MATCHING THE SIDEWALK WIDTH.

| REV. | DATE | DESCRIPTION | BY |
|------|------|-------------|----|
|      |      |             |    |
|      |      |             |    |
|      |      |             |    |



CITY OF BENTONVILLE  
 BENTONVILLE, ARKANSAS  
 WALTON BOULEVARD  
 AND MEDICAL CENTER  
 PARKWAY IMPROVEMENTS

TYPICAL SECTIONS

JOB NO.: 21T21080  
 DATE: JAN. 2022  
 DESIGNED BY: ZTM  
 DRAWN BY: TWM

BAR IS ONE INCH ON ORIGINAL DRAWING  
 0 1" IF NOT ONE INCH ON THIS SHEET, ADJUST SCALES ACCORDINGLY.

DRAWING NUMBER  
**C-105**

TSAUSTIN 1/26/2022 4:06:57 PM  
 WORKSPACE: Garver\_2012  
 \\garver\project\2021\21T21080 - Bentonville Walton Blvd-SE Med Ctr Imps\Drawings\WMC\_C101\_TS.dgn  
 REVISION DATE:



© 2022 GARVER, LLC  
 THIS DOCUMENT, ALONG WITH THE IDEAS AND DESIGNS CONVEYED HEREIN, SHALL BE CONSIDERED INSTRUMENTS OF PROFESSIONAL SERVICE AND ARE PROPERTY OF GARVER, LLC. ANY USE, REPRODUCTION, OR DISTRIBUTION OF THIS DOCUMENT, ALONG WITH THE IDEAS AND DESIGN CONTAINED HEREIN, IS PROHIBITED UNLESS AUTHORIZED IN WRITING BY GARVER, LLC OR EXPLICITLY ALLOWED IN THE GOVERNING PROFESSIONAL SERVICES AGREEMENT FOR THIS WORK.

**PRELIMINARY PLANS  
 FOR REVIEW ONLY  
 NOT FOR CONSTRUCTION**

| REV. | DATE | DESCRIPTION | BY |
|------|------|-------------|----|
|      |      |             |    |
|      |      |             |    |
|      |      |             |    |



**CITY OF BENTONVILLE**  
 BENTONVILLE, ARKANSAS

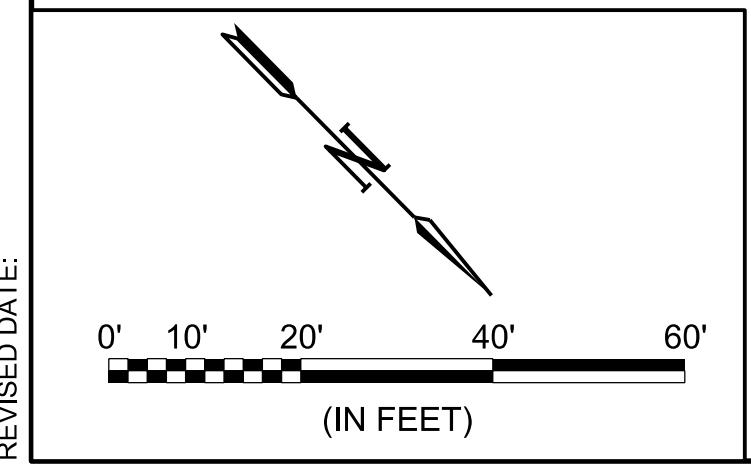
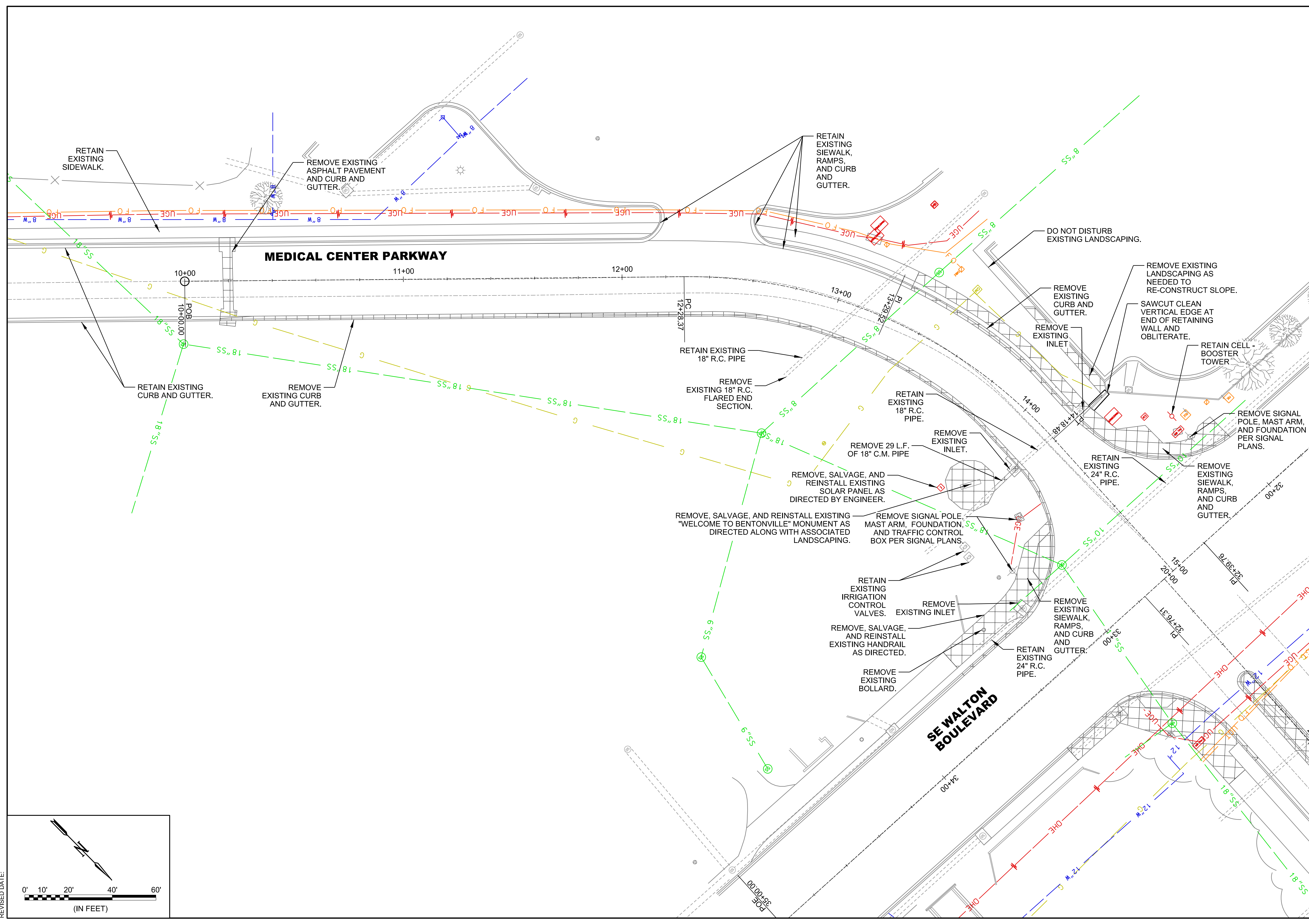
**WALTON BOULEVARD  
 AND MEDICAL CENTER  
 PARKWAY IMPROVEMENTS**

DEMOLITION PLAN

JOB NO.: 21T21080  
 DATE: JAN. 2022  
 DESIGNED BY: ZTM  
 DRAWN BY: TWM

BAR IS ONE INCH ON ORIGINAL DRAWING  
 0 1" IF NOT ONE INCH ON THIS SHEET, ADJUST SCALES ACCORDINGLY.

DRAWING NUMBER  
**C-251**



TSAUSTIN 1/26/2022 4:07:02 PM  
 WORKSPACE:Garver\_2012  
 \\garver\projects\2021\21T21080 - Bentonville - Walton Blvd-SE Med Ctr Imps\Drawings\WMC\_C251\_D.mxd  
 REVISION DATE:







© 2022 GARVER, LLC  
 THIS DOCUMENT, ALONG WITH THE IDEAS AND DESIGNS CONVEYED HEREIN, SHALL BE CONSIDERED INSTRUMENTS OF PROFESSIONAL SERVICE AND ARE PROPERTY OF GARVER, LLC. ANY USE, REPRODUCTION, OR DISTRIBUTION OF THIS DOCUMENT, ALONG WITH THE IDEAS AND DESIGN CONTAINED HEREIN, IS PROHIBITED UNLESS AUTHORIZED IN WRITING BY GARVER, LLC OR EXPLICITLY ALLOWED IN THE GOVERNING PROFESSIONAL SERVICES AGREEMENT FOR THIS WORK.

PRELIMINARY PLANS  
 FOR REVIEW ONLY  
 NOT FOR CONSTRUCTION

| REV. | DATE | DESCRIPTION | BY |
|------|------|-------------|----|
|      |      |             |    |
|      |      |             |    |
|      |      |             |    |



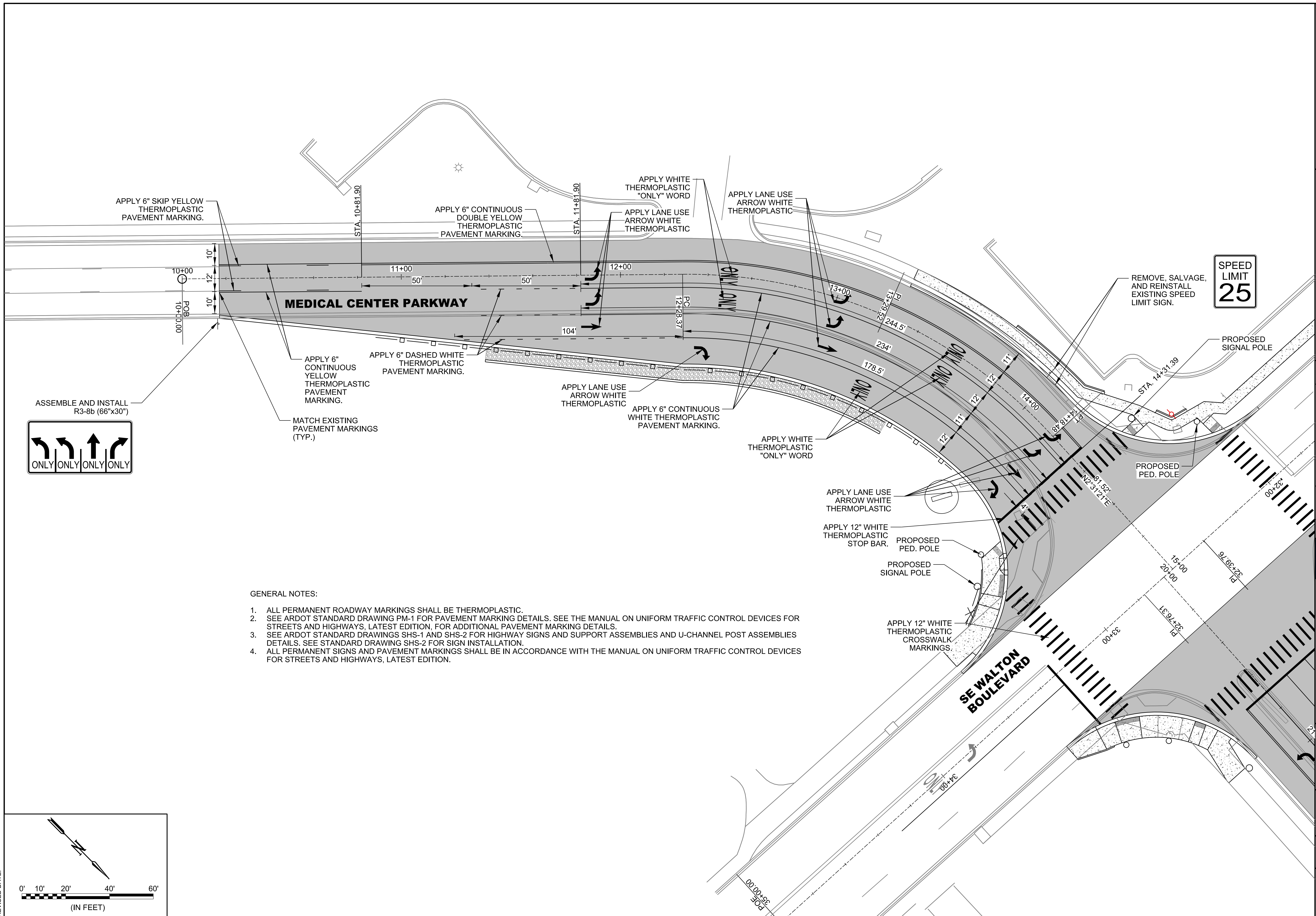
CITY OF BENTONVILLE  
 BENTONVILLE, ARKANSAS  
 WALTON BOULEVARD  
 AND MEDICAL CENTER  
 PARKWAY IMPROVEMENTS

PAVEMENT MARKING  
 AND SIGNAGE PLAN

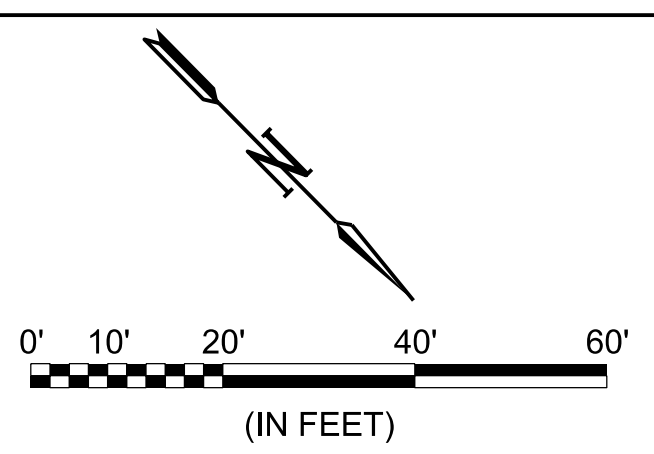
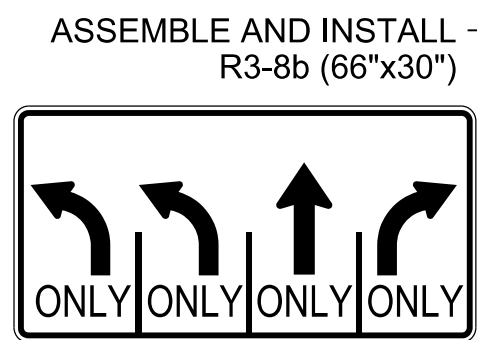
JOB NO.: 21T21080  
 DATE: JAN. 2022  
 DESIGNED BY: ZTM  
 DRAWN BY: TWB

BAR IS ONE INCH ON ORIGINAL DRAWING  
 IF NOT ONE INCH ON THIS SHEET, ADJUST SCALES ACCORDINGLY.

DRAWING NUMBER  
**C-301**



- GENERAL NOTES:
1. ALL PERMANENT ROADWAY MARKINGS SHALL BE THERMOPLASTIC.
  2. SEE ARDOT STANDARD DRAWING PM-1 FOR PAVEMENT MARKING DETAILS. SEE THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS, LATEST EDITION, FOR ADDITIONAL PAVEMENT MARKING DETAILS.
  3. SEE ARDOT STANDARD DRAWINGS SHS-1 AND SHS-2 FOR HIGHWAY SIGNS AND SUPPORT ASSEMBLIES AND U-CHANNEL POST ASSEMBLIES DETAILS. SEE STANDARD DRAWING SHS-2 FOR SIGN INSTALLATION.
  4. ALL PERMANENT SIGNS AND PAVEMENT MARKINGS SHALL BE IN ACCORDANCE WITH THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS, LATEST EDITION.



TSAUSTIN 1/26/2022 4:07:07 PM  
 WORKSPACE: Garver\_2012  
 P:\Projects\2021\21T21080 - Bentonville - Walton Blvd-SE Med Ctr Imps\Drawings\WMC\_C301\_P1.dgn  
 REVISION DATE:



© 2022 GARVER, LLC  
 THIS DOCUMENT, ALONG WITH THE IDEAS AND DESIGNS CONVEYED HEREIN, SHALL BE CONSIDERED INSTRUMENTS OF PROFESSIONAL SERVICE AND ARE PROPERTY OF GARVER, LLC. ANY USE, REPRODUCTION, OR DISTRIBUTION OF THIS DOCUMENT, ALONG WITH THE IDEAS AND DESIGN CONTAINED HEREIN, IS PROHIBITED UNLESS AUTHORIZED IN WRITING BY GARVER, LLC OR EXPLICITLY ALLOWED IN THE GOVERNING PROFESSIONAL SERVICES AGREEMENT FOR THIS WORK.

**PRELIMINARY PLANS  
 FOR REVIEW ONLY  
 NOT FOR CONSTRUCTION**

| REV. | DATE | DESCRIPTION | BY |
|------|------|-------------|----|
|      |      |             |    |
|      |      |             |    |
|      |      |             |    |



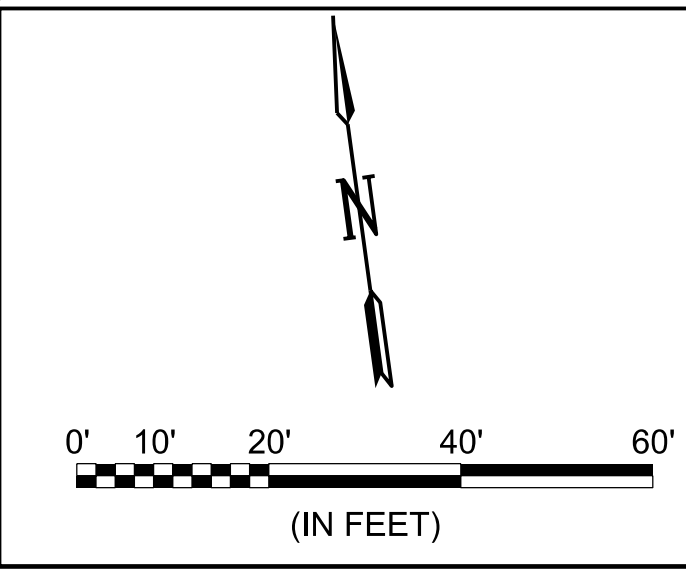
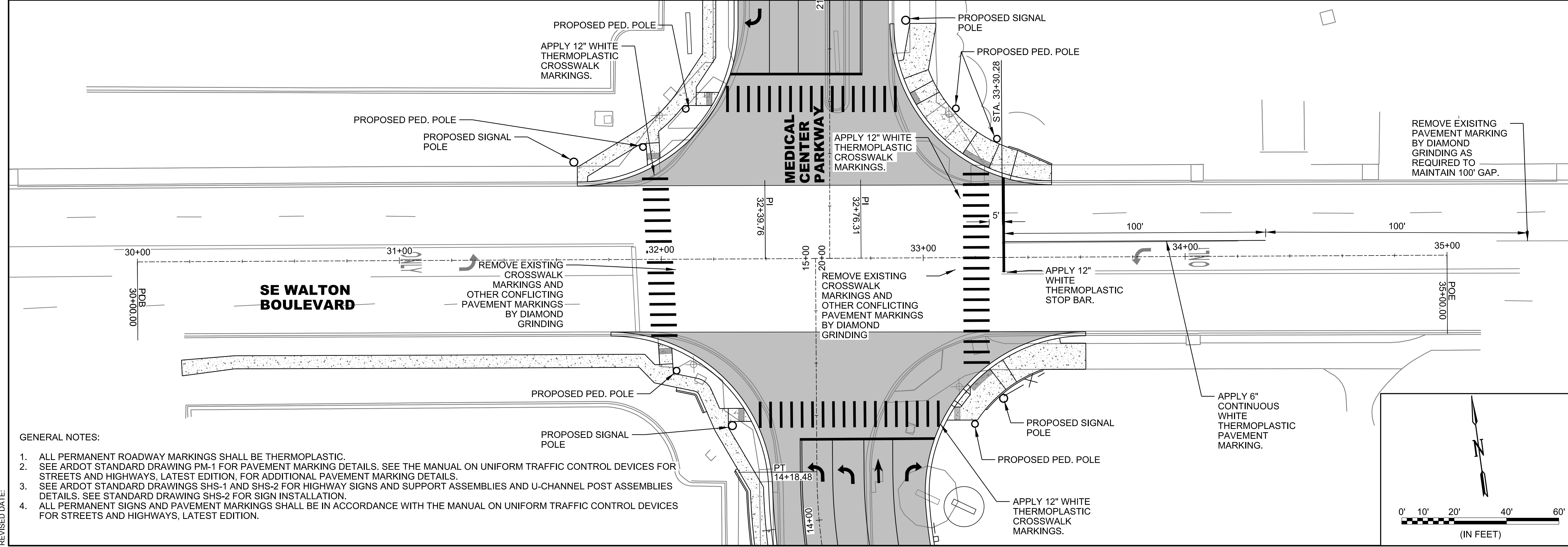
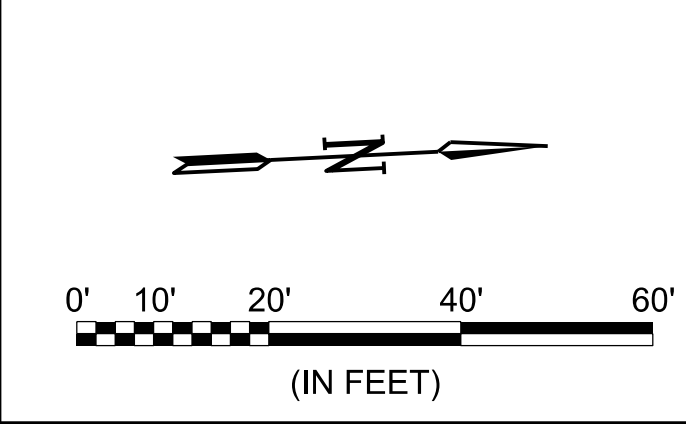
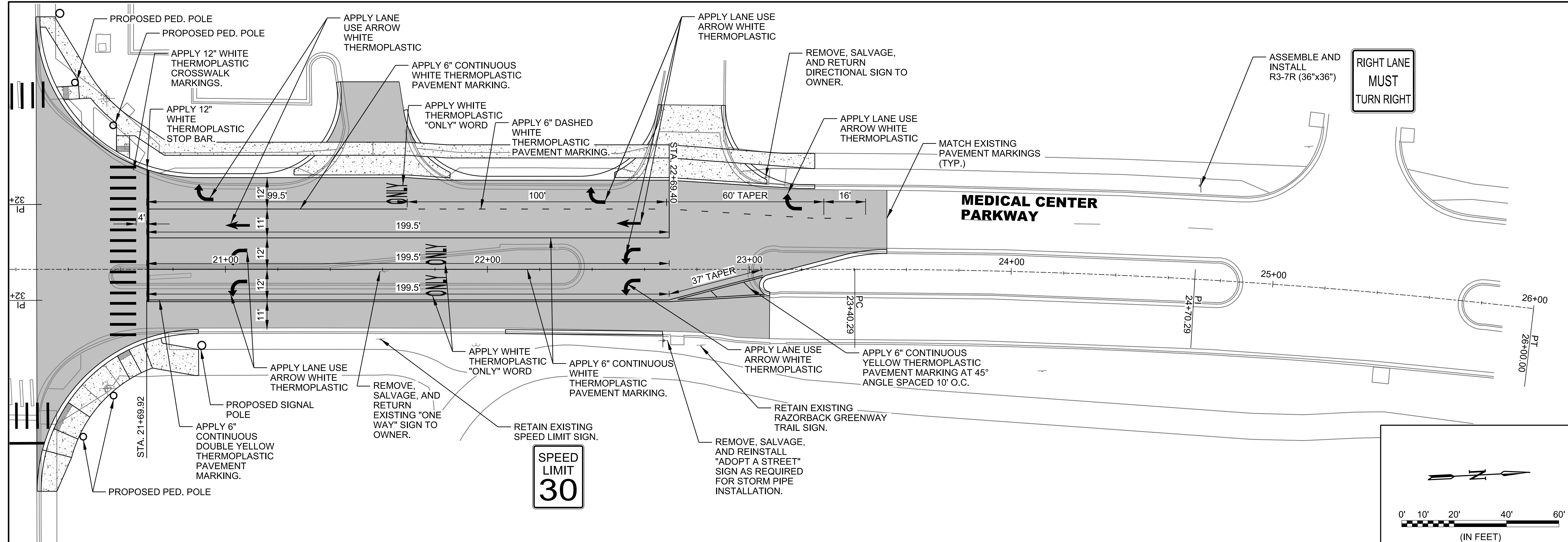
**CITY OF BENTONVILLE**  
 BENTONVILLE, ARKANSAS  
**WALTON BOULEVARD  
 AND MEDICAL CENTER  
 PARKWAY IMPROVEMENTS**

**PAVEMENT MARKING  
 AND SIGNAGE PLAN**

JOB NO.: 21T21080  
 DATE: JAN. 2022  
 DESIGNED BY: ZTM  
 DRAWN BY: TWMM

BAR IS ONE INCH ON ORIGINAL DRAWING  
 IF NOT ONE INCH ON THIS SHEET,  
 ADJUST SCALES ACCORDINGLY.

**DRAWING NUMBER  
 C-302**



- GENERAL NOTES:**
1. ALL PERMANENT ROADWAY MARKINGS SHALL BE THERMOPLASTIC.
  2. SEE ARDOT STANDARD DRAWING PM-1 FOR PAVEMENT MARKING DETAILS. SEE THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS, LATEST EDITION, FOR ADDITIONAL PAVEMENT MARKING DETAILS.
  3. SEE ARDOT STANDARD DRAWINGS SHS-1 AND SHS-2 FOR HIGHWAY SIGNS AND SUPPORT ASSEMBLIES AND U-CHANNEL POST ASSEMBLIES DETAILS. SEE STANDARD DRAWING SHS-2 FOR SIGN INSTALLATION.
  4. ALL PERMANENT SIGNS AND PAVEMENT MARKINGS SHALL BE IN ACCORDANCE WITH THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS, LATEST EDITION.

JAAdams 1/27/2022 9:55:42 AM  
 WORKSPACE:Garver\_2012  
 BentleyMapDataProjects\2021\21T21080 - Bentonville Walton Blvd-SE Med Ctr Imps\Drawings\WMC\_C302\_P1.dgn  
 REVISION DATE:



© 2022 GARVER, LLC  
 THIS DOCUMENT, ALONG WITH THE IDEAS AND DESIGNS CONVEYED HEREIN, SHALL BE CONSIDERED INSTRUMENTS OF PROFESSIONAL SERVICE AND ARE PROPERTY OF GARVER, LLC. ANY USE, REPRODUCTION, OR DISTRIBUTION OF THIS DOCUMENT, ALONG WITH THE IDEAS AND DESIGN CONTAINED HEREIN, IS PROHIBITED UNLESS AUTHORIZED IN WRITING BY GARVER, LLC OR EXPLICITLY ALLOWED IN THE GOVERNING PROFESSIONAL SERVICES AGREEMENT FOR THIS WORK.

**PRELIMINARY PLANS  
 FOR REVIEW ONLY  
 NOT FOR CONSTRUCTION**

| REV. | DATE | DESCRIPTION | BY |
|------|------|-------------|----|
|      |      |             |    |
|      |      |             |    |
|      |      |             |    |



**CITY OF BENTONVILLE**  
 BENTONVILLE, ARKANSAS

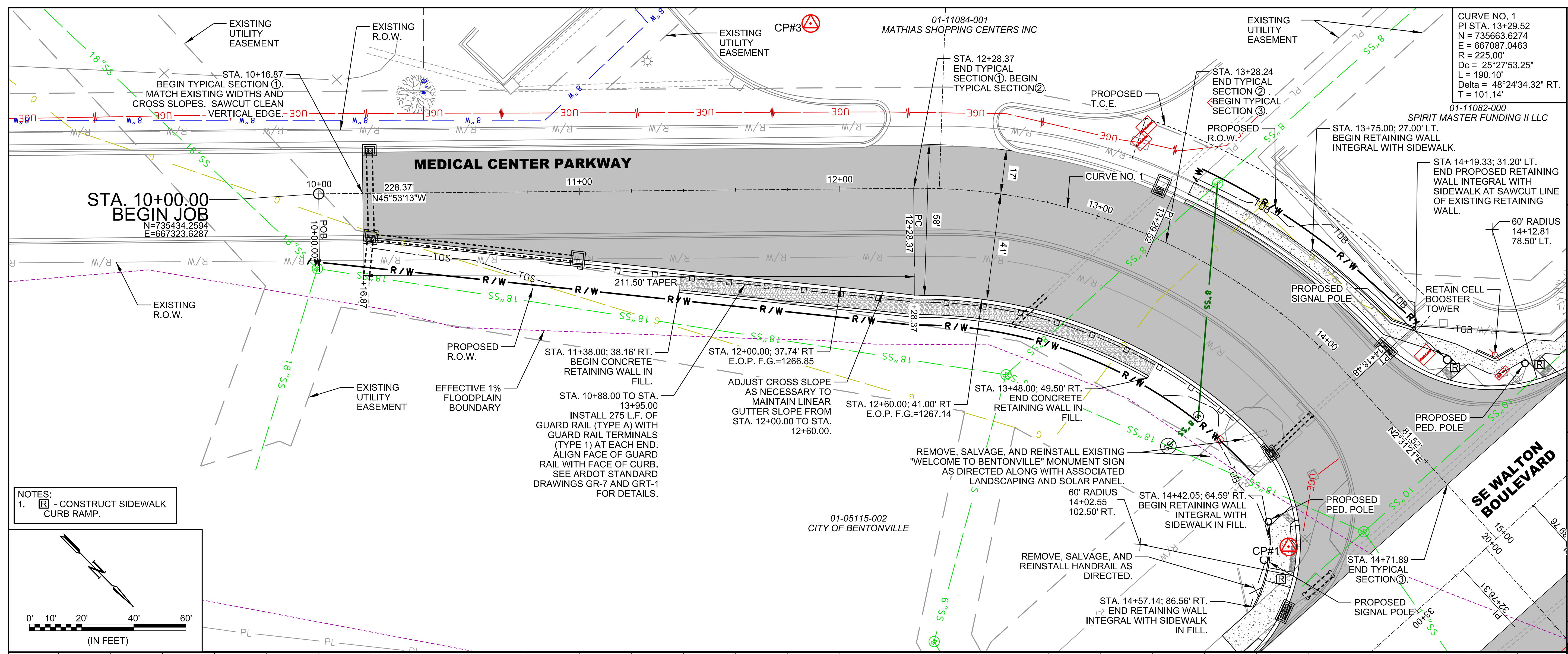
**WALTON BOULEVARD  
 AND MEDICAL CENTER  
 PARKWAY IMPROVEMENTS**

**(S) MEDICAL CENTER  
 PARKWAY PLAN AND  
 PROFILE**

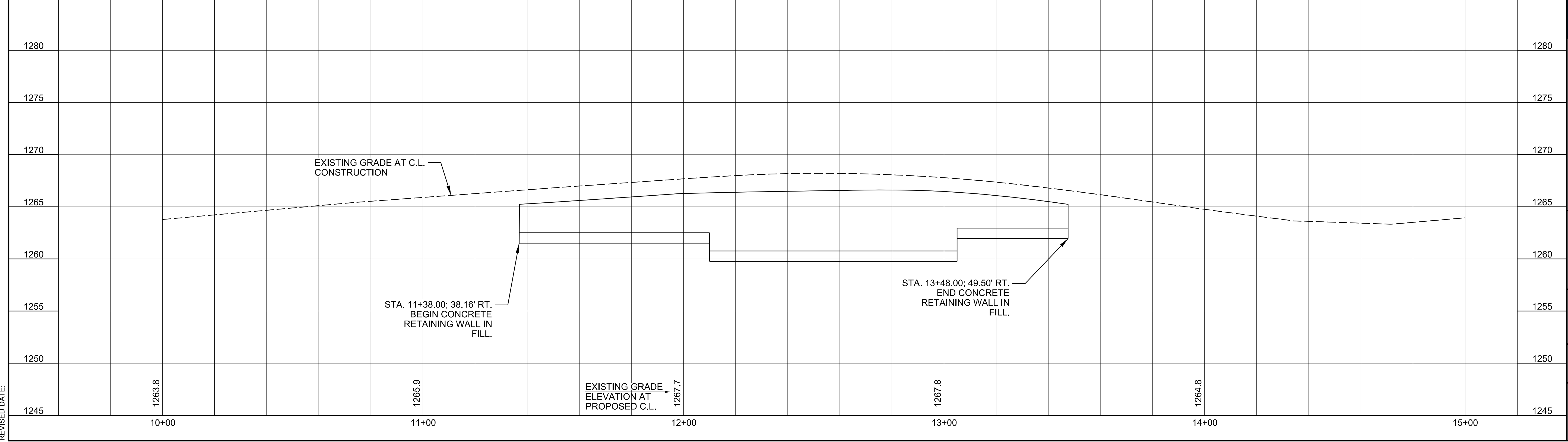
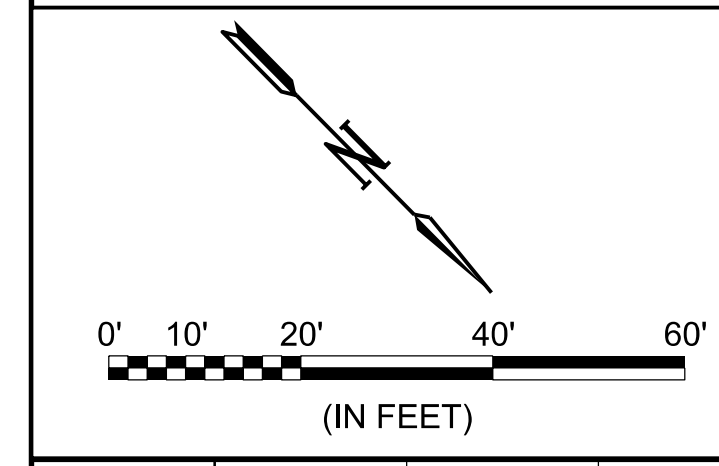
JOB NO.: 21T21080  
 DATE: JAN. 2022  
 DESIGNED BY: ZTM  
 DRAWN BY: TWM

BAR IS ONE INCH ON ORIGINAL DRAWING  
 IF NOT ONE INCH ON THIS SHEET, ADJUST SCALES ACCORDINGLY.

DRAWING NUMBER  
**C-501**

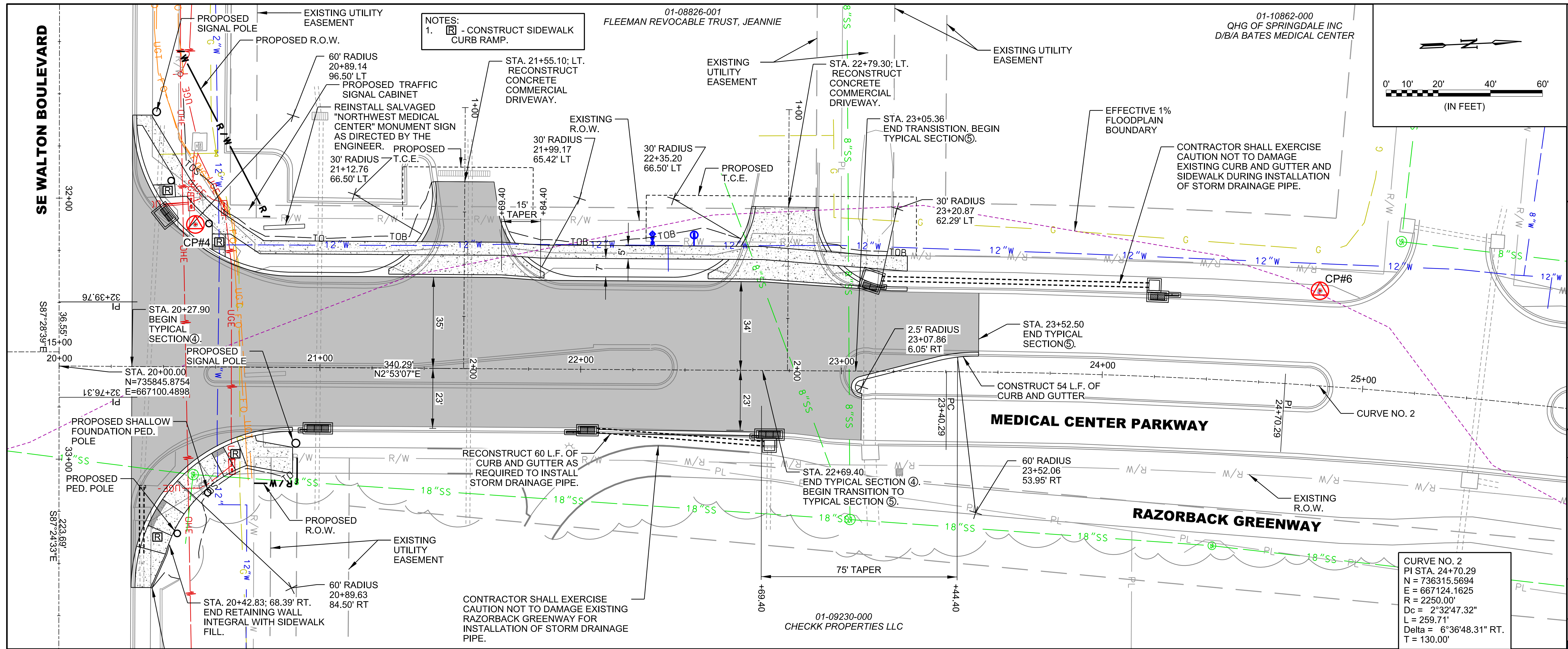


**NOTES:**  
 1. [Symbol] - CONSTRUCT SIDEWALK CURB RAMP.



TSAUSTIN 1/26/2022 4:07:14 PM  
 WORKSPACE:Garver\_2012  
 \\garver\projects\2021\21T21080 - Bentonville Walton Blvd-SE Med Ctr Imps\Drawings\WMC\_C501\_PP.dgn  
 REVISION DATE:

1/27/2022 3:38:30 PM  
 WORKSPACE: Garver\_2012  
 BENTONVILLE WALTON BLVD SE Med Ctr Imps Drawings\WMC\_CS602\_PP.dgn  
 REVISION DATE:

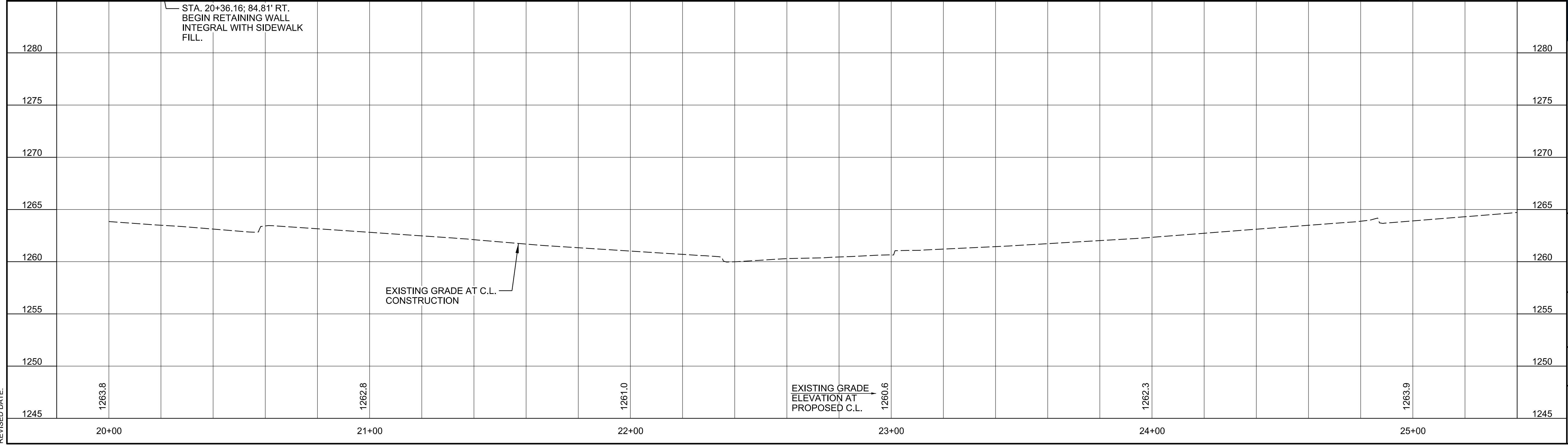


**GARVER**

© 2022 GARVER, LLC  
 THIS DOCUMENT, ALONG WITH THE IDEAS AND DESIGNS CONVEYED HEREIN, SHALL BE CONSIDERED INSTRUMENTS OF PROFESSIONAL SERVICE AND ARE PROPERTY OF GARVER, LLC. ANY USE, REPRODUCTION, OR DISTRIBUTION OF THIS DOCUMENT, ALONG WITH THE IDEAS AND DESIGNS CONTAINED HEREIN, IS PROHIBITED UNLESS AUTHORIZED IN WRITING BY GARVER, LLC OR EXPLICITLY ALLOWED IN THE GOVERNING PROFESSIONAL SERVICES AGREEMENT FOR THIS WORK.

**PRELIMINARY PLANS  
 FOR REVIEW ONLY  
 NOT FOR CONSTRUCTION**

| REV. | DATE | DESCRIPTION | BY |
|------|------|-------------|----|
|      |      |             |    |
|      |      |             |    |
|      |      |             |    |



**CITY OF BENTONVILLE**  
 BENTONVILLE, ARKANSAS

**WALTON BOULEVARD AND MEDICAL CENTER PARKWAY IMPROVEMENTS**

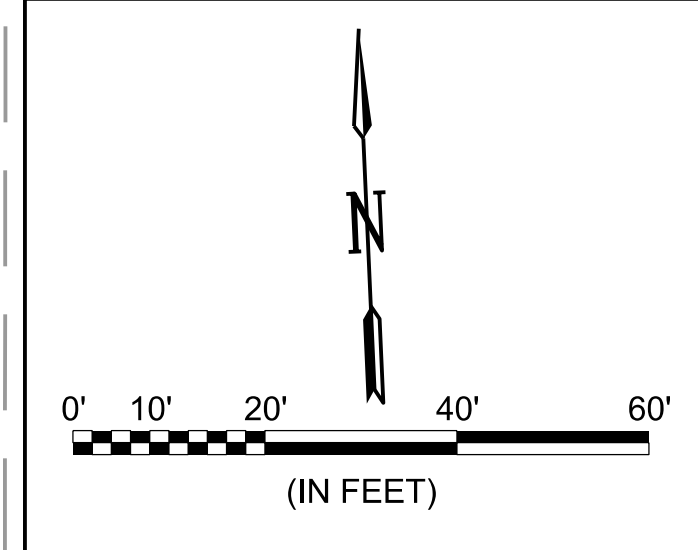
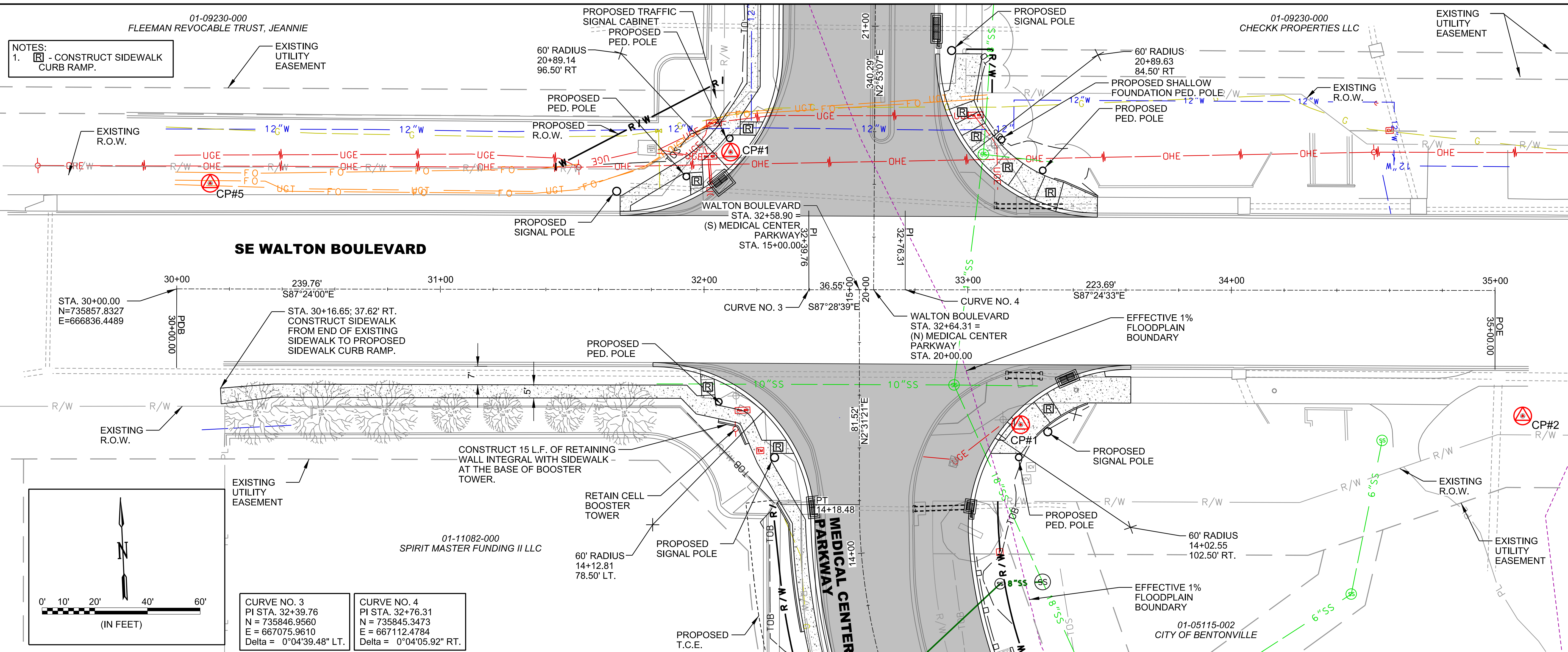
**(N) MEDICAL CENTER PARKWAY PLAN AND PROFILE**

JOB NO.: 21T21080  
 DATE: JAN. 2022  
 DESIGNED BY: ZTM  
 DRAWN BY: TWM

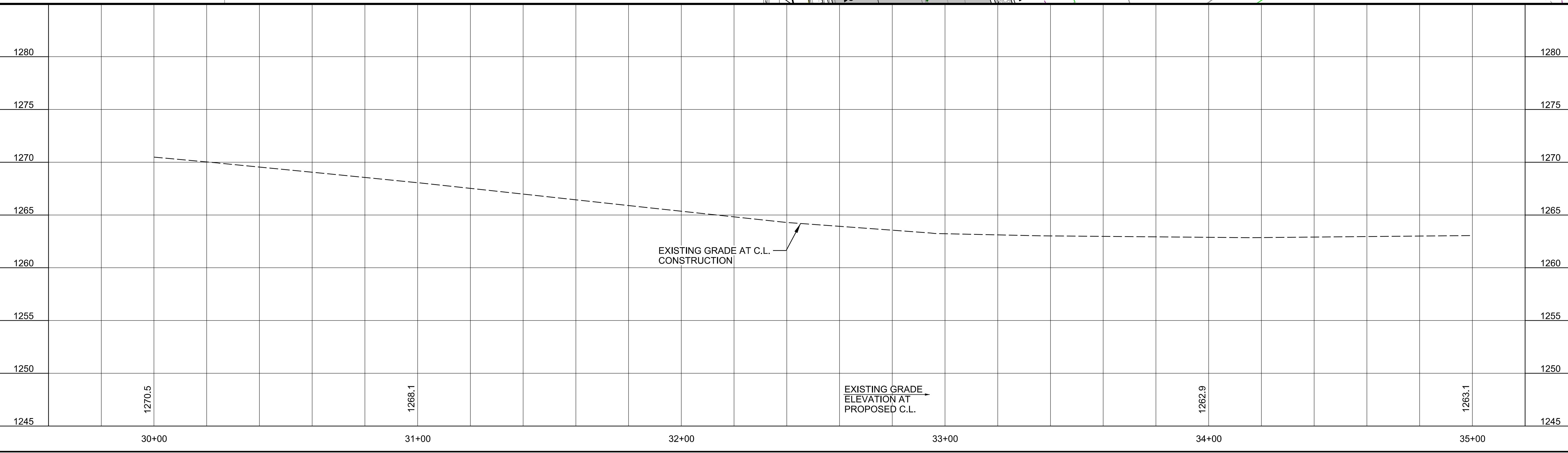
BAR IS ONE INCH ON ORIGINAL DRAWING  
 IF NOT ONE INCH ON THIS SHEET, ADJUST SCALES ACCORDINGLY.

**DRAWING NUMBER  
 C-502**

1/27/2022 3:39:13 PM  
 WORKSPACE:Garver\_2012  
 01-09230-000 - Bentonville Walton Blvd-SE  
 REVISION DATE: 01-09-2022  
 W:\Projects\2022\01-09230-000\Drawings\WMC\_C503\_PP.dgn



|   |   |
|---|---|
| <b>CURVE NO. 3</b><br>PI STA. 32+39.76<br>N = 735846.9560<br>E = 667075.9610<br>Delta = 0°04'39.48" LT. | <b>CURVE NO. 4</b><br>PI STA. 32+76.31<br>N = 735845.3473<br>E = 667112.4784<br>Delta = 0°04'05.92" RT. |
|---|---|



© 2022 GARVER, LLC  
 THIS DOCUMENT, ALONG WITH THE IDEAS AND DESIGNS CONVEYED HEREIN, SHALL BE CONSIDERED INSTRUMENTS OF PROFESSIONAL SERVICE AND ARE PROPERTY OF GARVER, LLC. ANY USE, REPRODUCTION, OR DISTRIBUTION OF THIS DOCUMENT, ALONG WITH THE IDEAS AND DESIGN CONTAINED HEREIN, IS PROHIBITED UNLESS AUTHORIZED IN WRITING BY GARVER, LLC OR EXPLICITLY ALLOWED IN THE GOVERNING PROFESSIONAL SERVICES AGREEMENT FOR THIS WORK.

**PRELIMINARY PLANS FOR REVIEW ONLY NOT FOR CONSTRUCTION**

| REV. | DATE | DESCRIPTION | BY |
|------|------|-------------|----|
|      |      |             |    |
|      |      |             |    |
|      |      |             |    |

**CITY OF BENTONVILLE**  
 BENTONVILLE, ARKANSAS

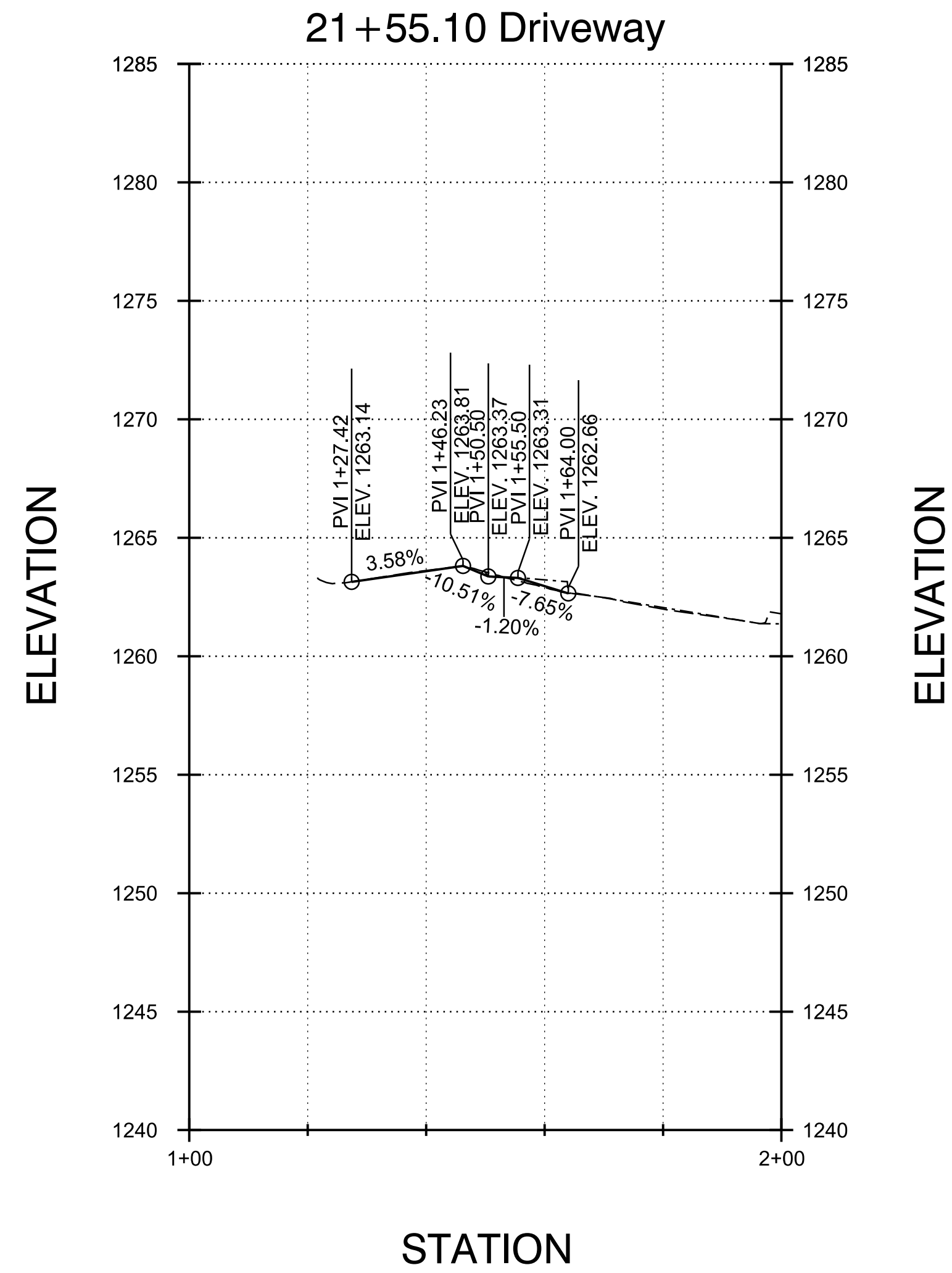
**WALTON BOULEVARD AND MEDICAL CENTER PARKWAY IMPROVEMENTS**

**SE WALTON BOULEVARD PLAN AND PROFILE**

JOB NO.: 21T21080  
 DATE: JAN. 2022  
 DESIGNED BY: ZTM  
 DRAWN BY: TWM

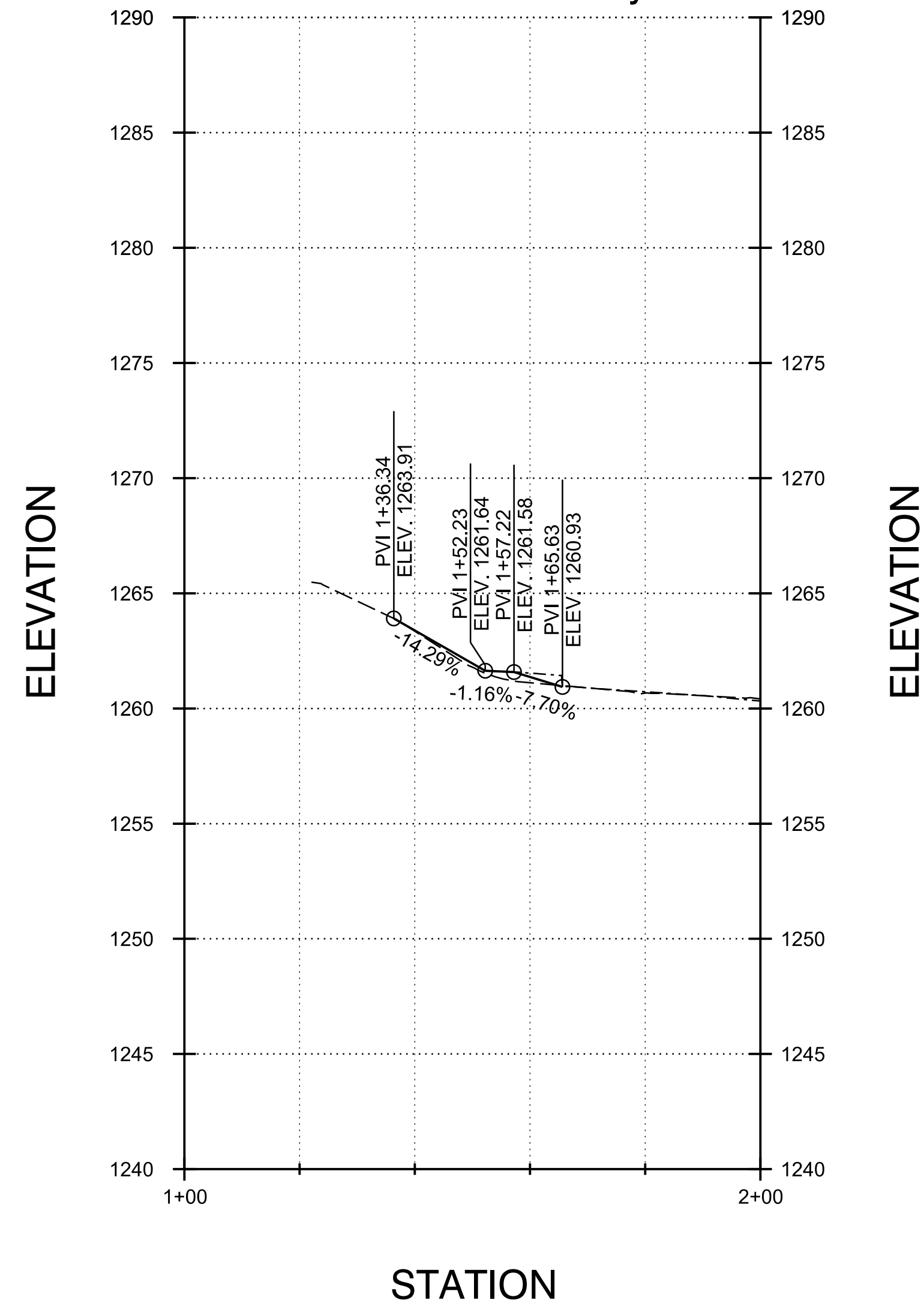
BAR IS ONE INCH ON ORIGINAL DRAWING  
 IF NOT ONE INCH ON THIS SHEET, ADJUST SCALES ACCORDINGLY.

**DRAWING NUMBER**  
**C-503**



ELEVATION

**22 + 79.30 Driveway**



ELEVATION

ELEVATION



© 2022 GARVER, LLC  
 THIS DOCUMENT, ALONG WITH THE  
 IDEAS AND DESIGNS CONVEYED  
 HEREIN, SHALL BE CONSIDERED  
 INSTRUMENTS OF PROFESSIONAL  
 SERVICE AND ARE PROPERTY OF  
 GARVER, LLC. ANY USE,  
 REPRODUCTION, OR DISTRIBUTION  
 OF THIS DOCUMENT, ALONG WITH  
 THE IDEAS AND DESIGN CONTAINED  
 HEREIN, IS PROHIBITED UNLESS  
 AUTHORIZED IN WRITING BY  
 GARVER, LLC OR EXPLICITLY  
 ALLOWED IN THE GOVERNING  
 PROFESSIONAL SERVICES  
 AGREEMENT FOR THIS WORK.

**PRELIMINARY PLANS  
 FOR REVIEW ONLY  
 NOT FOR CONSTRUCTION**

| REV. | DATE | DESCRIPTION | BY |
|------|------|-------------|----|
|      |      |             |    |
|      |      |             |    |
|      |      |             |    |



**CITY OF BENTONVILLE**  
 BENTONVILLE, ARKANSAS  
**WALTON BOULEVARD  
 AND MEDICAL CENTER  
 PARKWAY IMPROVEMENTS**

**DRIVEWAY PROFILES**

JOB NO.: 21T21080  
 DATE: JAN. 2022  
 DESIGNED BY: ZTM  
 DRAWN BY: TWMM

BAR IS ONE INCH ON ORIGINAL DRAWING  
 0' 1" IF NOT ONE INCH ON THIS SHEET, ADJUST SCALES ACCORDINGLY.

DRAWING NUMBER  
**C-521**



© 2022 GARVER, LLC  
THIS DOCUMENT, ALONG WITH THE IDEAS AND DESIGNS CONVEYED HEREIN, SHALL BE CONSIDERED INSTRUMENTS OF PROFESSIONAL SERVICE AND ARE PROPERTY OF GARVER, LLC. ANY USE, REPRODUCTION, OR DISTRIBUTION OF THIS DOCUMENT, ALONG WITH THE IDEAS AND DESIGN CONTAINED HEREIN, IS PROHIBITED UNLESS AUTHORIZED IN WRITING BY GARVER, LLC OR EXPLICITLY ALLOWED IN THE GOVERNING PROFESSIONAL SERVICES AGREEMENT FOR THIS WORK.

**PRELIMINARY PLANS  
FOR REVIEW ONLY  
NOT FOR CONSTRUCTION**

| REV. | DATE | DESCRIPTION | BY |
|------|------|-------------|----|
|      |      |             |    |
|      |      |             |    |
|      |      |             |    |
|      |      |             |    |



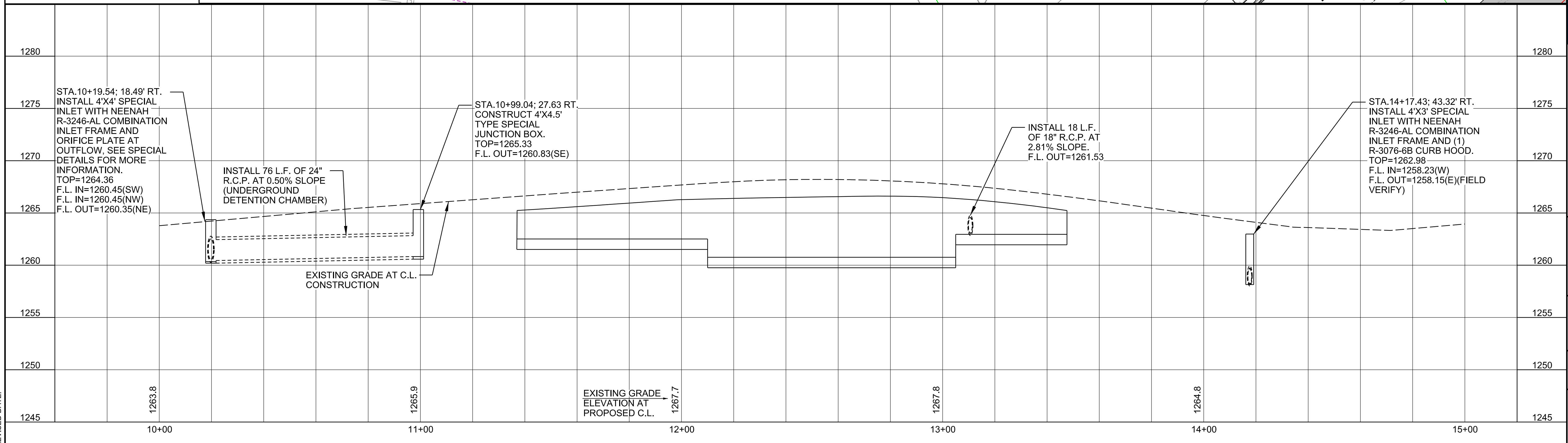
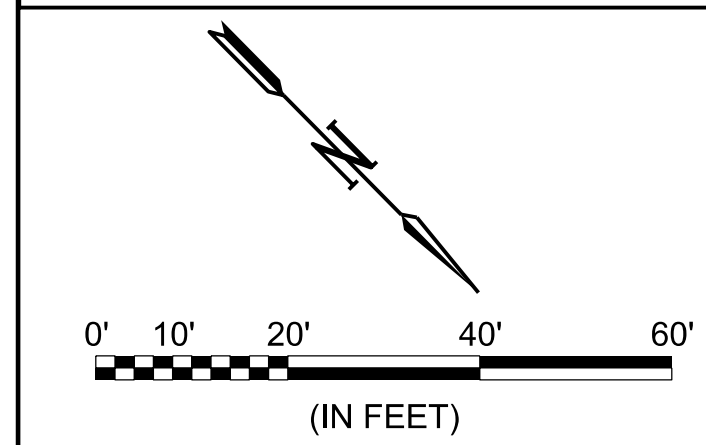
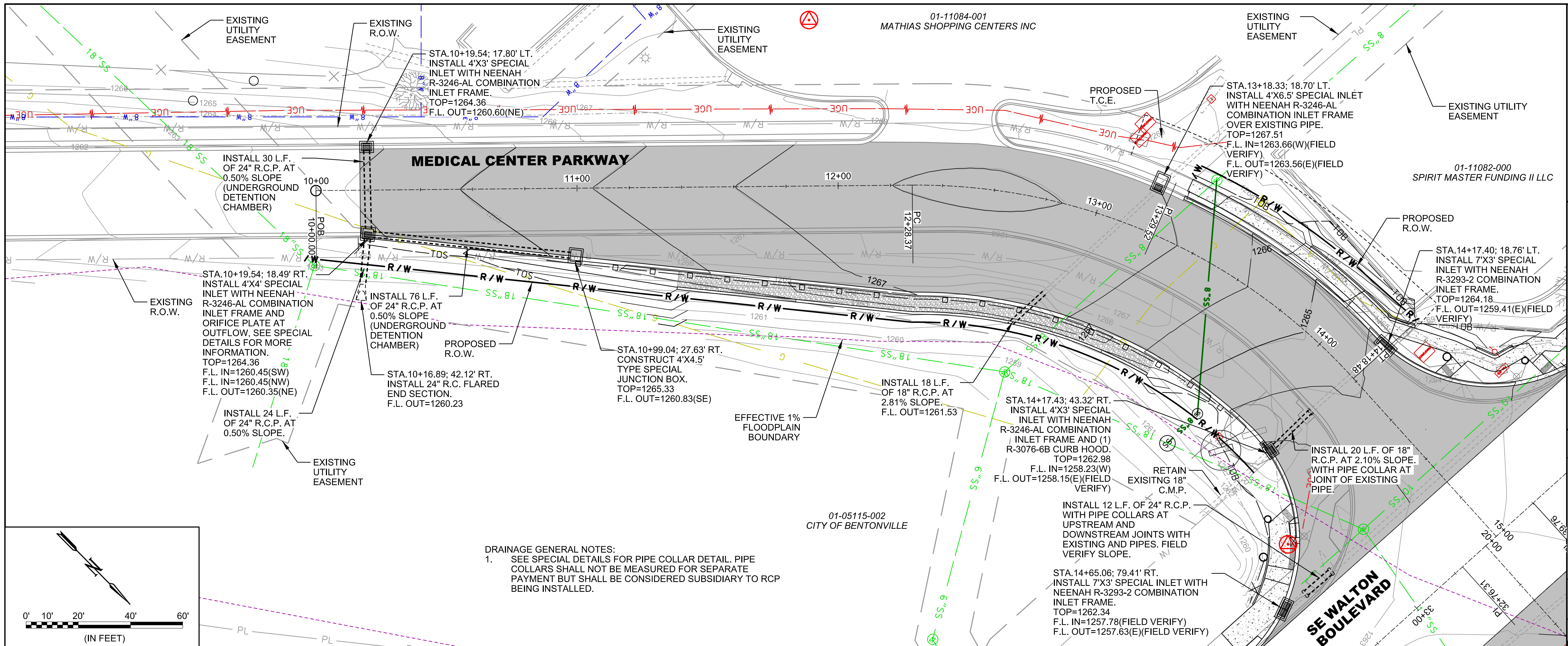
**CITY OF BENTONVILLE**  
BENTONVILLE, ARKANSAS  
**WALTON BOULEVARD  
AND MEDICAL CENTER  
PARKWAY IMPROVEMENTS**

**(S) MEDICAL CENTER  
PARKWAY DRAINAGE  
PLAN AND PROFILE**

JOB NO.: 21T21080  
DATE: JAN. 2022  
DESIGNED BY: ZTM  
DRAWN BY: TWMM

BAR IS ONE INCH ON ORIGINAL DRAWING  
IF NOT ONE INCH ON THIS SHEET, ADJUST SCALES ACCORDINGLY.

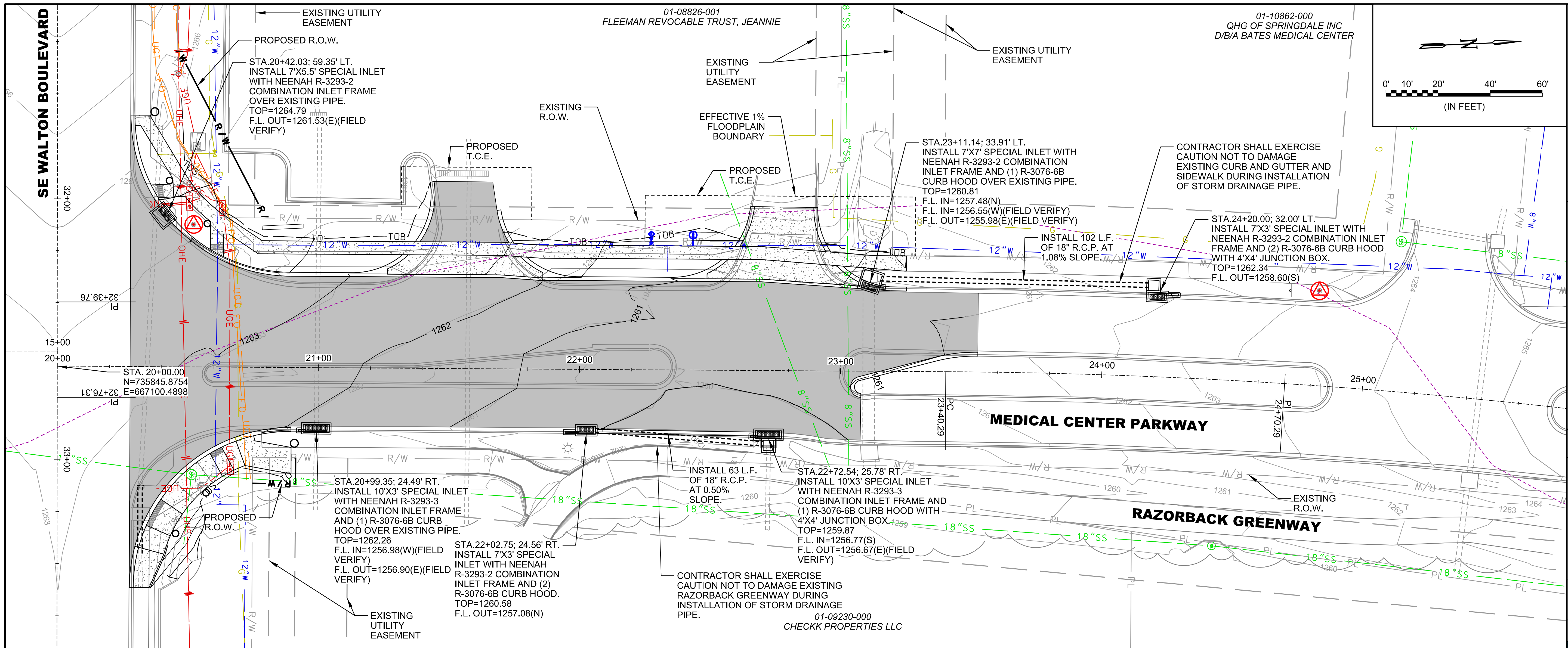
DRAWING NUMBER  
**C-551**



J:\Adams WORKSPACE\Garver\_2012\Bentonia\Drawings\Projects\2021\21T21080 - Bentonville Walton Blvd-SE Med Ctr Imps\Drawings\WMC\_C551\_DR.dgn  
 1/27/2022 9:55:47 AM  
 REVISION DATE:

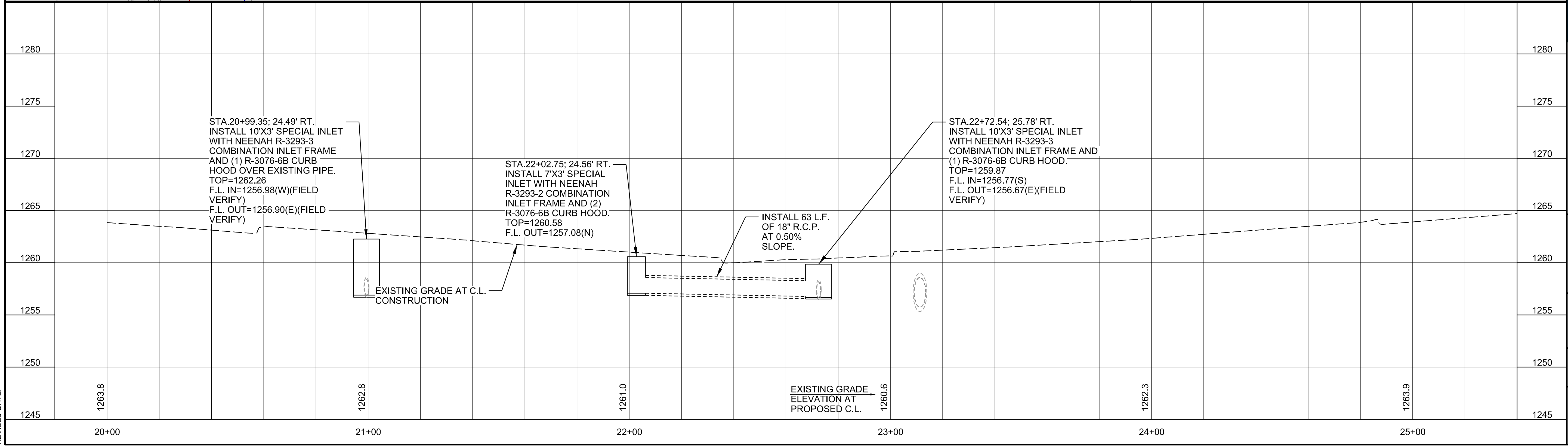


1/27/2022 3:40:13 PM  
 WORKSPACE:Garver\_2012  
 BENTONVILLE WALTON BLVD+SE  
 REVISION DATE:



**GARVER**  
 © 2022 GARVER, LLC  
 THIS DOCUMENT, ALONG WITH THE IDEAS AND DESIGNS CONVEYED HEREIN, SHALL BE CONSIDERED INSTRUMENTS OF PROFESSIONAL SERVICE AND ARE PROPERTY OF GARVER, LLC. ANY USE, REPRODUCTION, OR DISTRIBUTION OF THIS DOCUMENT, ALONG WITH THE IDEAS AND DESIGNS CONTAINED HEREIN, IS PROHIBITED UNLESS AUTHORIZED IN WRITING BY GARVER, LLC OR EXPLICITLY ALLOWED IN THE GOVERNING PROFESSIONAL SERVICES AGREEMENT FOR THIS WORK.

**PRELIMINARY PLANS FOR REVIEW ONLY NOT FOR CONSTRUCTION**



| REV. | DATE | DESCRIPTION | BY |
|------|------|-------------|----|
|      |      |             |    |
|      |      |             |    |
|      |      |             |    |

**CITY OF BENTONVILLE**  
 BENTONVILLE, ARKANSAS

**WALTON BOULEVARD AND MEDICAL CENTER PARKWAY IMPROVEMENTS**

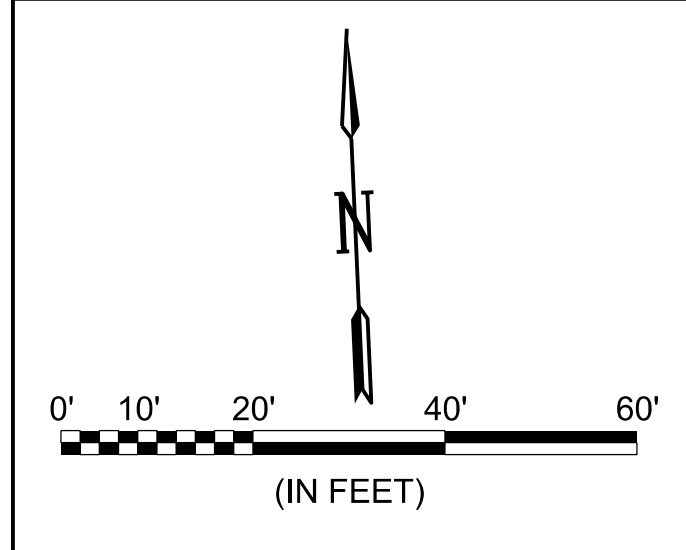
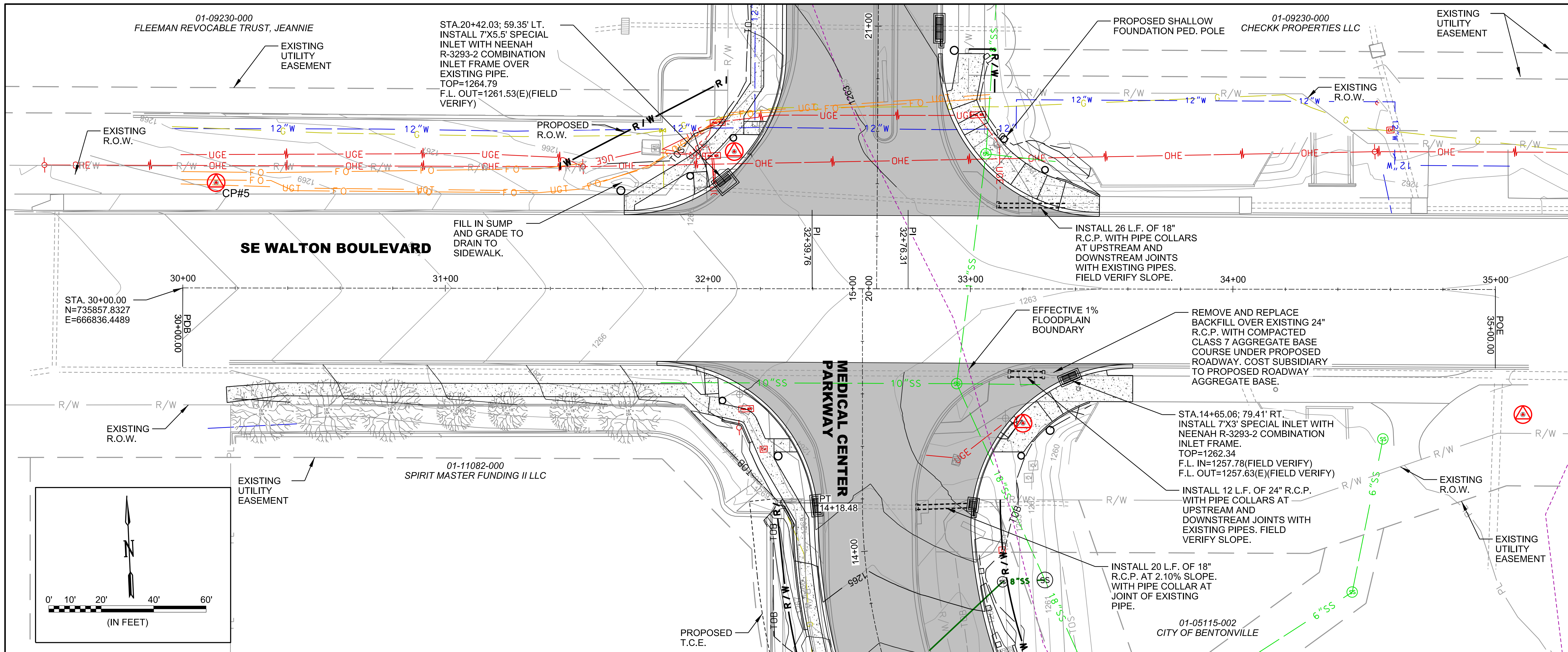
(N) MEDICAL CENTER PARKWAY DRAINAGE PLAN AND PROFILE

JOB NO.: 21T21080  
 DATE: JAN. 2022  
 DESIGNED BY: ZTM  
 DRAWN BY: TWM

BAR IS ONE INCH ON ORIGINAL DRAWING  
 IF NOT ONE INCH ON THIS SHEET, ADJUST SCALES ACCORDINGLY.

DRAWING NUMBER  
**C-552**

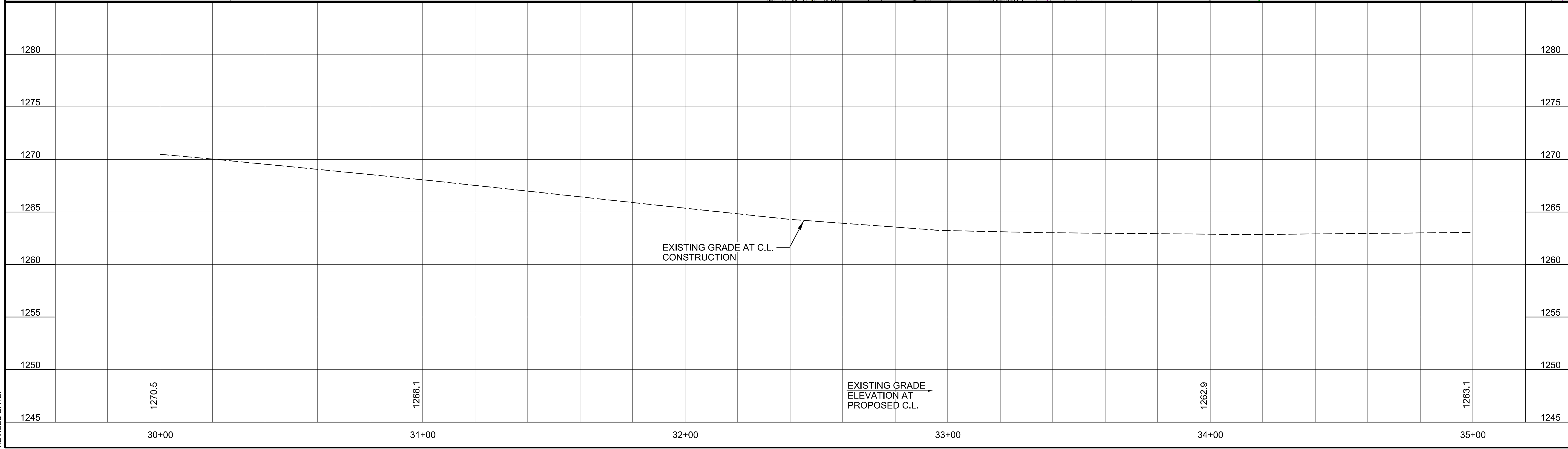
1/27/2022 3:42:20 PM  
 WORKSPACE:Garver\_2012  
 PROJECT:01-09230-000 - Bentonville Walton Blvd-SE  
 DRAWING DATE: 01-09-2022  
 REVISION DATE:



**GARVER**  
 © 2022 GARVER, LLC  
 THIS DOCUMENT, ALONG WITH THE IDEAS AND DESIGNS CONVEYED HEREIN, SHALL BE CONSIDERED INSTRUMENTS OF PROFESSIONAL SERVICE AND ARE PROPERTY OF GARVER, LLC. ANY USE, REPRODUCTION, OR DISTRIBUTION OF THIS DOCUMENT, ALONG WITH THE IDEAS AND DESIGN CONTAINED HEREIN, IS PROHIBITED UNLESS AUTHORIZED IN WRITING BY GARVER, LLC OR EXPLICITLY ALLOWED IN THE GOVERNING PROFESSIONAL SERVICES AGREEMENT FOR THIS WORK.

**PRELIMINARY PLANS  
 FOR REVIEW ONLY  
 NOT FOR CONSTRUCTION**

| REV. | DATE | DESCRIPTION | BY |
|------|------|-------------|----|
|      |      |             |    |
|      |      |             |    |
|      |      |             |    |



**CITY OF BENTONVILLE**  
 BENTONVILLE, ARKANSAS

**WALTON BOULEVARD  
 AND MEDICAL CENTER  
 PARKWAY IMPROVEMENTS**

**SE WALTON BOULEVARD DRAINAGE  
 PLAN AND PROFILE**

JOB NO.: 21T21080  
 DATE: JAN. 2022  
 DESIGNED BY: ZTM  
 DRAWN BY: TWM

BAR IS ONE INCH ON ORIGINAL DRAWING  
 IF NOT ONE INCH ON THIS SHEET, ADJUST SCALES ACCORDINGLY.

**DRAWING NUMBER  
 C-553**

**TRAFFIC SIGNAL NOTES:**

- ALL ELECTRICAL WORK SHALL BE PERFORMED IN ACCORDANCE WITH THE CURRENT EDITIONS OF THE NFPA 70 (CURRENT EDITION) NATIONAL ELECTRICAL CODE, NFPA 101 (CURRENT EDITION) LIFE SAFETY CODE, STATE ELECTRICAL CODE AND LOCAL ELECTRICAL CODE.
- EXTEND GREEN EQUIPMENT GROUNDING CONDUCTOR (E.G.C.) FROM GROUND BAR AT MAIN BREAKER TO CONTROL PANEL AND TO FIRST POLE. SOLIDLY BOND E.G.C. TO GROUND LUG OF CONTROL CABINET AND TO POLE GROUND. ENSURE THAT ONLY ONE NEUTRAL-TO-GROUND BOND EXISTS IN THE SYSTEM AND THAT IT IS AT THE MAIN BREAKER.
- ELECTRICAL SERVICE SHALL BE PROVIDED BY THE CITY/COUNTY TO A SERVICE POLE WITH EXTERNAL RAINTIGHT BREAKER (MAIN BREAKER), GALVANIZED STEEL SERVICE RISER, METER LOOP (IF REQUIRED), AND WEATHERHEAD AT A MUTUALLY ACCEPTABLE POINT WITHIN THE RIGHT-OF-WAY. IF THE SERVICE POINT IS OVER 10 FEET FROM THE CONTROLLER, THE CONTRACTOR SHALL PROVIDE AND INSTALL A SEPARATE TWO CIRCUIT EXTERNAL BREAKER (SECONDARY BREAKER) ON OR NEAR THE TRAFFIC SIGNAL CONTROLLER CABINET AND SHALL INSTALL CONDUIT, ELECTRICAL SERVICE WIRE (2c#6 A.W.G. USE RATED, WITH GROUND TYPICAL), AND PERFORM WIRING TO TAP INTO THE CITY/ COUNTY'S MAIN BREAKER AS PART OF THIS CONTRACT. CONDUIT IS PAID FOR AS A SEPARATE ITEM OF THIS CONTRACT. TWO CIRCUIT BREAKERS, CONSIDERED SUBSIDIARY TO THE CONTROL EQUIPMENT, ARE NEEDED WHERE STREET LIGHTING IS INCLUDED. AS PART OF THE SIGNAL INSTALLATION, STREET LIGHTING CIRCUIT (2c#12 A.W.G. UF RATED, TYPICAL) SHALL BE KEPT FROM THE CIRCUIT SERVING THE TRAFFIC SIGNAL CONTROL EQUIPMENT FROM THE POINT OF TIE-IN AT THE SECONDARY BREAKER PROVIDED BY THE
- CONTRACTOR SHALL CONNECT A SEPARATE NEUTRAL FOR EACH LOAD SWITCH REPRESENTED ON EACH SIGNAL POLE.
- TRAFFIC CONTROLLER CABINET AND LAYOUT SHALL BE SUCH THAT IT IS NOT NECESSARY TO SHUT DOWN POWER OR REMOVE LOAD SWITCHES IN ORDER TO EASILY TEST OR MODIFY DETECTOR INPUTS TO THE CONTROLLER.
- CONTROLLER CABINET SHALL BE WIRED SUCH THAT DURING FLASH OPERATIONS POWER TO THE LOAD SWITCHES CANNOT BACKFEED TO LOAD SWITCH POWER BUSS.
- ALL PARTS OF THIS INSTALLATION SHALL BE IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION, STANDARD DRAWINGS, AND WITH THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES, CURRENT EDITION.
- CONDUIT INSTALLED UNDER ROADWAY SURFACES SHALL BE INSTALLED BY PUSHING OR BORING METHODS. IF THE ENGINEER DETERMINES THIS IS NOT FEASIBLE, THEN A TRENCHING METHOD AS SHOWN IN THE STANDARD DRAWINGS MAY BE USED.
- TRAFFIC SIGNAL POLES SHALL BE GALVANIZED. BACKPLATES SHALL BE SUPPLIED FOR ALL SIGNAL HEADS.
- PAVEMENT MARKINGS SHOWN FOR REFERENCE ONLY. SEE PERMANENT PAVEMENT MARKING DETAILS.
- FOUNDATION FOR ALL POLES SHALL BE EXTENDED IF NECESSARY TO ACCOMMODATE THE REQUIREMENTS FOR SIGNAL HEAD CLEARANCE ABOVE ROADWAY ONLY AT LOCATIONS WHERE THE GROUND ELEVATION AT THE POLE IS BELOW THE ELEVATION OF THE ROADWAY (SEE NOTES ON STANDARD DRAWING). PAYMENT WILL BE INCLUDED IN SECTION 714 TRAFFIC SIGNAL MAST ARM AND POLE WITH FOUNDATION OF THE STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION, CURRENT EDITION.
- ALL CONCRETE PULL BOXES SHALL BE (TYPE 3 HD) UNLESS OTHERWISE INDICATED. ALL CONDUIT SHALL BE THREE (3") INCH DIAMETER UNLESS SPECIFIED ON PLANS.
- CONTRACTOR SHALL NOTIFY ALL EXISTING UTILITY OWNERS BEFORE BEGINNING WORK ON THIS PROJECT.
- LED LUMINAIRE ASSEMBLIES SHALL HAVE A BUG RATING OF U0.
- HARDWARE INPUTS MAY BE DETERMINED BY SUPPLIER. EACH DETECTOR OUTPUT SHALL INPUT THE CONTROLLER THROUGH A SEPARATE INPUT UNLESS OTHERWISE NOTED AND BE PROGRAMMED TO ACTUATE THE ASSOCIATED PHASE. COMBINATION (COMB.) DETECTORS SHALL ALSO BE PROGRAMMED TO PROVIDE VEHICLE COUNT/OCCUPANCY DATA.
- THE LOCAL RADIO WITH ANTENNA SHALL BE COMPATIBLE WITH THE EXISTING CLOSED LOOP COORDINATION SYSTEM IN THE CITY/COUNTY.
- TO DETERMINE UTILITY CLEARANCES ABOVE THE TRAFFIC SIGNAL POLE, REFER TO THE POLE SCHEDULE FOR VERTICAL SHAFT HEIGHT. WHERE THE POLE SCHEDULE INDICATES THAT A LUMINAIRE ARM WILL BE USED, THIRTY-EIGHT (38') FEET SHOULD BE USED TO DETERMINE UTILITY CLEARANCE ABOVE THE LUMINAIRE ARM. WHERE THE POLE SCHEDULE INDICATES A TRAFFIC SIGNAL POLE WITHOUT A LUMINAIRE ARM, A HEIGHT OF TWENTY-ONE (21') FEET SHOULD BE USED TO DETERMINE UTILITY CLEARANCE ABOVE THE TRAFFIC SIGNAL MAST ARM. AN ADDITIONAL SIX (6') FEET SHOULD BE USED DIRECTLY ABOVE "VIDEO DETECTOR" AT LOCATIONS SHOWN ON THE SIGNAL PLANS.
- THE DESIRABLE MINIMUM DISTANCE FROM THE FACE OF ROADWAY CURB OR SHOULDER EDGE TO THE FACE OF NON-BREAKAWAY POLE OR OBSTRUCTION IS SIX (6') FEET. REFER TO TRAFFIC SIGNAL PLANS FOR SPECIFIC LOCATION OF POLES, CONTROLLER AND ANY OTHER NON-BREAKAWAY OBSTRUCTIONS. REFER TO "DESIGN PARAMETERS, MINIMUM CLEAR ZONE DISTANCE" FOR MINIMUM DISTANCE FROM THE EDGE OF TRAVELED WAY TO THE FACE OF A NON-BREAKAWAY POLE OR OBSTRUCTION. TRAFFIC SIGNAL POLES OR ANY OTHER NON-BREAKAWAY OBSTRUCTION SHALL NOT BE INSTALLED WITHIN THE CLEAR ZONE.
- AS DETERMINED BY THE ENGINEER, FOUNDATION EMBEDMENT MAY BE DECREASED BY A MAXIMUM OF TWO FEET IF COMPETENT ROCK IS ENCOUNTERED PRIOR TO ACHIEVING PLAN EMBEDMENT AND AT LEAST HALF OF THE REMAINING PLAN EMBEDMENT LENGTH IS KEYED INTO COMPETENT ROCK.
- CONNECTION OF TRAFFIC SIGNAL DISPLAY TO FIELD WIRING SHALL UTILIZE AN APPROVED TERMINAL STRIP BEHIND HAND-HOLE COVER AT BASE OF POLE. TERMINAL STRIP SHALL PROVIDE PROTECTION TO PREVENT EXPOSURE TO THE PUBLIC IN THE EVENT THAT POLE COVER IS MISSING. PAYMENT FOR TERMINAL STRIPS SHALL BE INCLUDED IN ITEM 714 TRAFFIC SIGNAL MAST ARM AND POLE WITH FOUNDATION OF THE STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION, CURRENT EDITION.
- CONTROLLER CABINET LAYOUT AND ORIENTATION SHALL CONFORM TO IMSA STANDARDS.
- ONE VIDEO PROGRAMMING MODULE SHALL BE PROVIDED FOR AIMING AND SETUP OF DETECTORS IF THE VIDEO SYSTEM CANNOT BE ADJUSTED THROUGH HARDWARE AND SOFTWARE PROVIDED BY ITEMS WITHIN THE JOB.
- TRAFFIC SIGNAL CONTRACTOR SHALL NOTIFY THE RESIDENT ENGINEER OR ASSIGNED DEPARTMENT PROJECT INSPECTOR EACH DAY PRIOR TO SIGNAL RELATED WORK. NO WORK ON TRAFFIC SIGNALS WILL BE ALLOWED OR APPROVED WITHOUT THIS PRIOR NOTIFICATION.
- ALL STEEL POLES SHALL BE DESIGNED TO MEET THE AASHTO STANDARD SPECIFICATIONS FOR STRUCTURAL SUPPORTS FOR HIGHWAY SIGNS, LUMINAIRES AND TRAFFIC SIGNALS, 4th EDITION (2001) WITH 2003 AND 2006 INTERIMS.
- DOOR PANEL TEST PUSH BUTTONS SHALL ACTUATE INDICATED PHASES. DETECTOR ASSIGNMENTS AND/OR SIDE PANEL JUMPERS MAY REQUIRE MODIFICATION.
- ALL SYSTEM DETECTOR RACKS AND ASSOCIATED EQUIPMENT SHALL BE PROTECTED BY THE MAIN CONTROLLER CABINET POWER SURGE PROTECTION.
- IN PULL BOXES, POLE BASES, JUNCTION BOXES, AND CONTROLLER CABINETS, THE DIRECTION OF EACH CABLE RUN SHALL BE INDICATED BY ATTACHING A PERMANENT TAG OF RIGID PLASTIC OR NON-FERROUS METAL TO THE CONDUIT. TAGS SHALL BE EMBOSSED, STAMPED OR ENGRAVED WITH LETTERS 1/4" OR GREATER IN HEIGHT AND SECURED TO THE CONDUIT WITH NYLON OR PLASTIC TIES. IN INSTANCES WHERE THE CONDUIT OR CONDUIT ENTRANCES ARE NOT VISIBLE OR ACCESSIBLE, A DIRECTION TAG SHALL BE ATTACHED TO
- THE CONTRACTOR SHALL PERFORM ALL WORK POSSIBLE THAT WILL MINIMIZE THE TIME THAT THE TRAFFIC SIGNAL IS OUT OF OPERATION. IF, IN THE OPINION OF THE ENGINEER, TRAFFIC CONDITIONS WARRANT, THE CONTRACTOR SHALL PROVIDE FLAGMEN TO DIRECT TRAFFIC WHILE THE TRAFFIC SIGNAL IS OUT OF OPERATION.
- ALL NON-METALLIC CONDUIT RUNS SHALL HAVE BELL RING FITTINGS INSTALLED ON THE TERMINATING ENDS OF THE CONDUIT. THIS INCLUDES PULL BOXES, POLE BASES, AND TRAFFIC SIGNAL CABINETS.
- ALL CONCRETE PULL BOXES SHALL BE SET ON A GRAVEL OR CRUSHED STONE BEDDING AS SPECIFIED IN SECTION 711, CONCRETE PULL BOX, OF THE STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION, EDITION OF 2014.
- THE CONTROLLER SHALL BE SIEMENS EAGLE M62 CONTROLLER CONSISTENT AND COMPATIBLE WITH THE CITY OF BENTONVILLE EXISTING EQUIPMENT, SOFTWARE, HARDWARE,
- VIDEO DETECTION SHALL BE ITERIS VANTAGE NEXT VIDEO DETECTION. MUST BE FULLY COMPATIBLE WITH EXISTING EQUIPMENT, SOFTWARE, AND NETWORK.
- VIDEO CABLE SHALL BE AS RECOMMENDED BY THE MANUFACTURER OF THE VIDEO PROCESSOR UNIT AND COMPATIBLE WITH THE SPECIFIED VIDEO DETECTORS.
- TRAFFIC SIGNAL POLES, MAST ARMS, PEDESTRIAN POLES, AND LUMINAIRE ARMS SHALL BE PELCO DECORATIVE PER SPECIAL PROVISIONS.

- PEDESTRIAN SIGNAL HEADS SHALL BE BLACK CLAMSHELL TYPE HOUSING WITH POLARA APS PUSH BUTTON WITH SPECIAL MESSAGES.
- INSTALL AN EAGLE SIZE SUPER P CABINET (WITH INTEGRATED BBS COMPARTMENT) BLACK IN COLOR WITH FRONT, REAR, AND RIGHT SIDE OPENING DOORS.
- BATTERY BACKUP SYSTEM SHALL BE CLARY UPS SYSTEM OPERATING AS LINE FILTER FOR INCOMING SIGNALS. MUST BE FULLY COMPATIBLE WITH EXISTING EQUIPMENT, SOFTWARE, AND NETWORK.
- PREEMPTION SHALL BE GLOBAL TRAFFIC TECHNOLOGIES OPTICOM GPS PREEMPTION AND MUST BE FULLY COMPATIBLE WITH EXISTING EQUIPMENT, SOFTWARE, SERVER, AND
- LUMINAIRES SHALL BE HOLOPHANE MEMPHIS STYLE DECORATIVE LED LUMINAIRES.
- THE POSITION OF DETECTOR ZONES MAY BE FIELD ADJUSTED TO ACHIEVE MAXIMUM EFFICIENCY IN COUNT DATA AND VEHICLE ACTUATION, AS APPROVED BY THE ENGINEER.
- THERE SHALL BE NO DEVIATION FROM THIS PLAN WITHOUT PRIOR WRITTEN APPROVAL FROM THE ENGINEER.
- CONTRACTOR SHALL COORDINATE WITH T.E.C. INC., FOR COORDINATION TIMING DATA. CONTROLLER SHALL BE PROGRAMMED AND TESTED WITH COORDINATION DATA PRIOR TO
- CONTRACTOR SHALL INSTALL, TERMINATE, AND TEST FIBER FROM NEAREST SERVICE POINT INTO CABINET, PRIOR TO TURN ON.
- CITY OF BENTONVILLE WILL PROVIDE STREET NAME SIGNS AND THE CONTRACTOR SHALL INSTALL. CONTRACTOR SHALL PROVIDE AND INSTALL ALL OTHER SIGNS.
- ALL POLES, MAST ARMS, AND ACCESSORIES SHALL BE BLACK IN COLOR.
- CONTRACTOR SHALL REPAIR ALL SCUFFS, SCRATCHES, ETC., WITH CITY APPROVED MATERIALS.
- CONTRACTOR SHALL PROVIDE EQUIPMENT SUBMITTALS TO THE CITY OF BENTONVILLE FOR APPROVAL PRIOR TO PURCHASING.
- CONTRACTOR SHALL PROVIDE FINAL AS-BUILT DRAWINGS TO THE CITY OF BENTONVILLE.
- THE CITY OF BENTONVILLE RESERVES THE RIGHT TO REJECT ANY MATERIALS OR EQUIPMENT THAT HAS BEEN DAMAGED IN SHIPPING, HANDLING, OR UNLOADING AND MAY REQUIRE REPLACEMENT AT NO COST TO THE CITY.
- NEW TRAFFIC SIGNALS SHALL BE OPERATIONAL PRIOR TO REMOVAL OF EXISTING SIGNALS.
- TRAFFIC SIGNAL EQUIPMENT REMOVED FROM THE INTERSECTIONS SHALL BECOME THE PROPERTY OF THE CITY OF BENTONVILLE. (SEE SPECIAL PROVISION)

\* CLARIFICATION TO TRAFFIC SIGNAL NOTE 3:  
ELECTRICAL SERVICE SHALL BE INSTALLED BY THE CONTRACTOR AND THE CONTRACTOR TO COORDINATE THE SERVICE CONNECTION WITH THE ELECTRIC DEPARTMENT. ADDITIONALLY, FOR ELECTRICAL SERVICE, A FREE-STANDING RACK SIMILAR TO A DS-1003 STANDARD SHOULD BE INSTALLED PER THE BEUD.

**PERMANENT TRAFFIC SIGNAL QUANTITIES**

| ITEM NUMBER | ITEM  | QUANTITY | UNIT |
|-------------|---|----------|------|
| 55          | SYSTEM LOCAL CONTROLLER-FIBER (8 PHASES)                | 1        | EA   |
| 56          | ETHERNET SWITCH   | 1        | EA   |
| 57          | GPS PREEMPTION SYSTEM                                   | 1        | EA   |
| 58          | BATTERY BACKUP SYSTEM                                   | 1        | EA   |
| 59          | WIC FIBER ENCLOSURE                                     | 1        | EA   |
| 60          | TRAFFIC SIGNAL HEAD, LED, (3 SECTION, 1 WAY)            | 10       | EA   |
| 61          | TRAFFIC SIGNAL HEAD, LED, (4 SECTION, 1 WAY)            | 2        | EA   |
| 62          | TRAFFIC SIGNAL HEAD, LED, (5 SECTION, 1 WAY)            | 2        | EA   |
| 63          | CENTRAL CONTROL UNIT                                    | 1        | EA   |
| 64          | POLE MOUNTED ASSEMBLY                                   | 8        | EA   |
| 65          | INFRARED PROGRAMMING DEVICE                             | 1        | EA   |
| 66          | COUNTDOWN PEDESTRIAN SIGNAL HEAD, LED                   | 8        | EA   |
| 67          | TRAFFIC SIGNAL CABLE (5C/14 A.W.G.)                     | 2908     | LF   |
| 68          | TRAFFIC SIGNAL CABLE (7C/14 A.W.G.)                     | 264      | LF   |
| 69          | TRAFFIC SIGNAL CABLE (20C/14 A.W.G.)                    | 736      | LF   |
| 70          | ELECTRICAL CONDUCTORS-IN-CONDUIT (1C/8 A.W.G., E.G.C.)  | 826      | LF   |
| 71          | ELECTRICAL CONDUCTORS-IN-CONDUIT (1C/12 A.W.G., E.G.C.) | 220      | LF   |
| 72          | ELECTRICAL CONDUCTORS-IN-CONDUIT (2C/6 A.W.G.)          | 37       | LF   |
| 73          | ELECTRICAL CONDUCTORS FOR LUMINAIRES                    | 956      | LF   |
| 74          | COMMUNICATION CABLE, FIBER (12 CHANNEL)                 | 111      | LF   |
| 75          | GALVANIZED STEEL CONDUIT (2")                           | 14       | LF   |
| 76          | NON-METALLIC CONDUIT (2")                               | 111      | LF   |
| 77          | NON-METALLIC CONDUIT (3")                               | 831      | LF   |
| 79          | CONCRETE PULL BOX (TYPE 3)                              | 4        | EA   |
| 79          | CONCRETE PULL BOX (TYPE 1 HD)                           | 1        | EA   |
| 80          | CONCRETE PULL BOX (TYPE 3 HD)                           | 7        | EA   |
| ** 81       | PULL BOX FOR FIBER, DS-1609, 20"x48"x24"                | 2        | EA   |
| 82          | TRAFFIC SIGNAL MAST ARM AND POLE WITH FOUNDATION (36')  | 1        | EA   |
| 83          | TRAFFIC SIGNAL MAST ARM AND POLE WITH FOUNDATION (38')  | 1        | EA   |
| 84          | TRAFFIC SIGNAL MAST ARM AND POLE WITH FOUNDATION (48')  | 1        | EA   |
| 85          | TRAFFIC SIGNAL MAST ARM AND POLE WITH FOUNDATION (54')  | 1        | EA   |
| 86          | LED LUMINAIRE ASSEMBLY                                  | 4        | EA   |
| 87          | TRAFFIC SIGNAL PEDESTAL POLE WITH FOUNDATION            | 6        | EA   |
| 88          | SERVICE POINT ASSEMBLY (2 CIRCUITS)                     | 1        | EA   |
| ** 89       | 18" STREET NAME SIGN                                    | 4        | EA   |
| 90          | VIDEO DETECTOR RACK (16 CHANNEL)                        | 1        | EA   |
| * 91        | VIDEO DETECTOR (IP)                                     | 7        | EA   |
| 92          | VIDEO CABLE (EXTERIOR CAT 5E)                           | 1490     | LF   |
| 93          | VIDEO MONITOR (CLR)                                     | 1        | EA   |
| 94          | CENTRAL CONTROL UNIT (4 CHANNEL)                        | 2        | EA   |
| * 95        | VIDEO PROCESSOR, EDGE CARD IP (2 CAMERA)                | 5        | EA   |

\* ONE SPARE VIDEO DETECTOR AND ONE SPARE VIDEO PROCESSOR SHALL BE SUPPLIED  
\*\* PROVIDED BY BEUD, INSTALLED BY CONTRACTOR



© 2022 GARVER, LLC  
THIS DOCUMENT, ALONG WITH THE IDEAS AND DESIGNS CONVEYED HEREIN, SHALL BE CONSIDERED INSTRUMENTS OF PROFESSIONAL SERVICE AND ARE PROPERTY OF GARVER, LLC. ANY USE, REPRODUCTION, OR DISTRIBUTION OF THIS DOCUMENT, ALONG WITH THE IDEAS AND DESIGN CONTAINED HEREIN, IS PROHIBITED UNLESS AUTHORIZED IN WRITING BY GARVER, LLC OR EXPLICITLY ALLOWED IN THE GOVERNING PROFESSIONAL SERVICES AGREEMENT FOR THIS WORK.

**PRELIMINARY PLANS  
FOR REVIEW ONLY  
NOT FOR CONSTRUCTION**

| REV. | DATE | DESCRIPTION | BY |
|------|------|-------------|----|
|      |      |             |    |
|      |      |             |    |
|      |      |             |    |



**CITY OF BENTONVILLE**  
BENTONVILLE, ARKANSAS

**WALTON BOULEVARD  
AND MEDICAL CENTER  
PARKWAY IMPROVEMENTS**

SIGNAL NOTES AND QUANTITIES

JOB NO.: 21T21080  
DATE: JAN. 2022  
DESIGNED BY: CEM  
DRAWN BY: HRH

BAR IS ONE INCH ON ORIGINAL DRAWING  
0 1" IF NOT ONE INCH ON THIS SHEET, ADJUST SCALES ACCORDINGLY.

DRAWING NUMBER

**C-600**



© 2022 GARVER, LLC  
 THIS DOCUMENT, ALONG WITH THE IDEAS AND DESIGNS CONVEYED HEREIN, SHALL BE CONSIDERED INSTRUMENTS OF PROFESSIONAL SERVICE AND ARE PROPERTY OF GARVER, LLC. ANY USE, REPRODUCTION, OR DISTRIBUTION OF THIS DOCUMENT, ALONG WITH THE IDEAS AND DESIGN CONTAINED HEREIN, IS PROHIBITED UNLESS AUTHORIZED IN WRITING BY GARVER, LLC OR EXPLICITLY ALLOWED IN THE GOVERNING PROFESSIONAL SERVICES AGREEMENT FOR THIS WORK.

PRELIMINARY PLANS  
 FOR REVIEW ONLY  
 NOT FOR CONSTRUCTION

| BY | DESCRIPTION | DATE | REV. |
|----|-------------|------|------|
|    |             |      |      |
|    |             |      |      |



CITY OF BENTONVILLE  
 BENTONVILLE, ARKANSAS  
 WALTON BOULEVARD  
 AND MEDICAL CENTER  
 PARKWAY IMPROVEMENTS

SIGNALIZATION  
 PLANS

JOB NO.: 21T21080  
 DATE: JAN. 2022  
 DESIGNED BY: CEM  
 DRAWN BY: HRH

BAR IS ONE INCH ON ORIGINAL DRAWING  
 IF NOT ONE INCH ON THIS SHEET, ADJUST SCALES ACCORDINGLY.

DRAWING NUMBER  
**C-601**

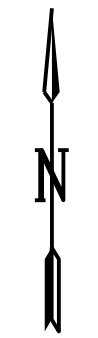
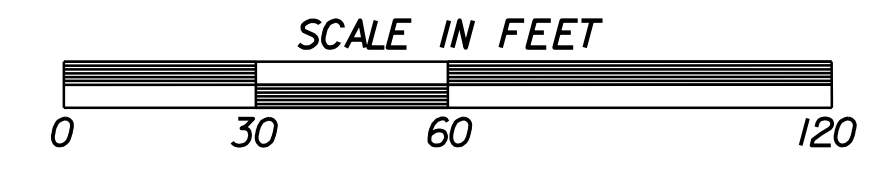
WALTON BLVD. AND MEDICAL CENTER PKWY.  
 POLE DIMENSIONS

| POLE | MAST ARM | *MAST ARM ANGLE | VERT. SHAFT | LUM. ARM | *LUM. ANGLE |
|------|----------|-----------------|-------------|----------|-------------|
| A    | 36'      | 270°            | 35'         | 15'      | 270°        |
| B    | -        | -               | 10'         | -        | -           |
| C    | -        | -               | 10'         | -        | -           |
| D    | 38'      | 180°            | 35'         | 15'      | 180°        |
| E    | -        | -               | 10'         | -        | -           |
| F    | -        | -               | 10'         | -        | -           |
| G    | 54'      | 180°            | 35'         | 15'      | 180°        |
| H    | -        | -               | 10'         | -        | -           |
| I    | 48'      | 180°            | 35'         | 15'      | 180°        |
| J    | -        | -               | 10'         | -        | -           |

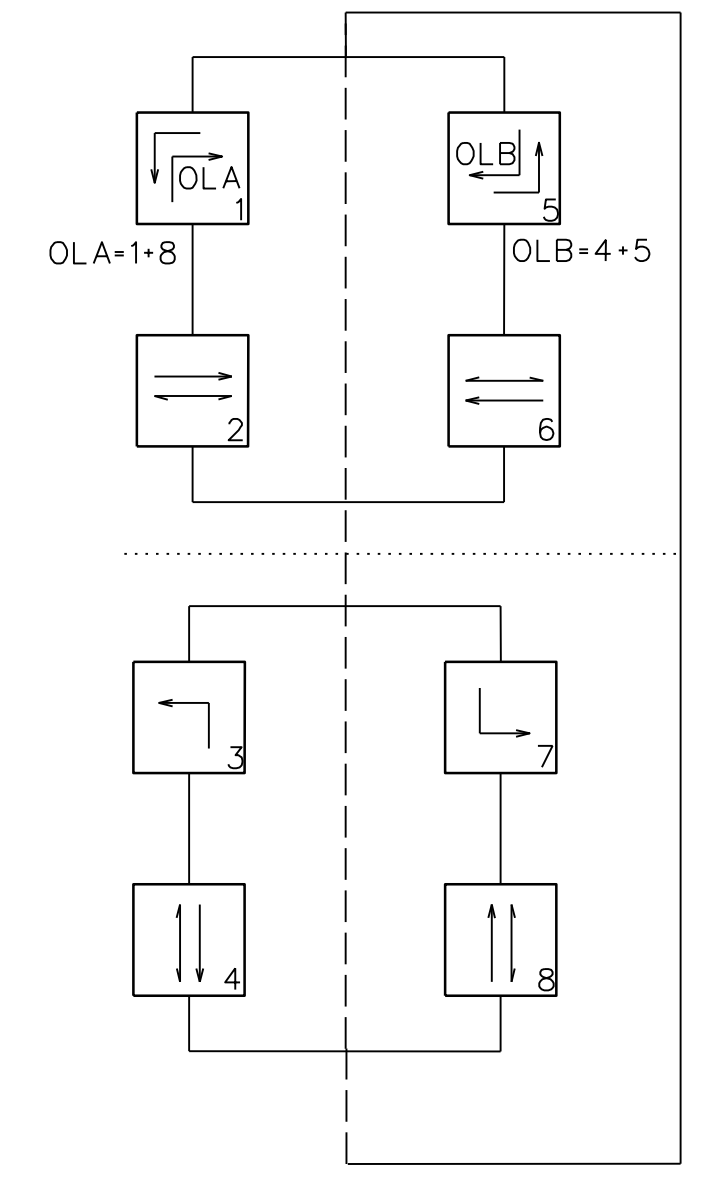
\* ANGLE MEASURED CLOCKWISE FROM HAND HOLE.

DETECTOR SPACING CHART  
 WALTON BLVD. VIRTUAL LOOPS

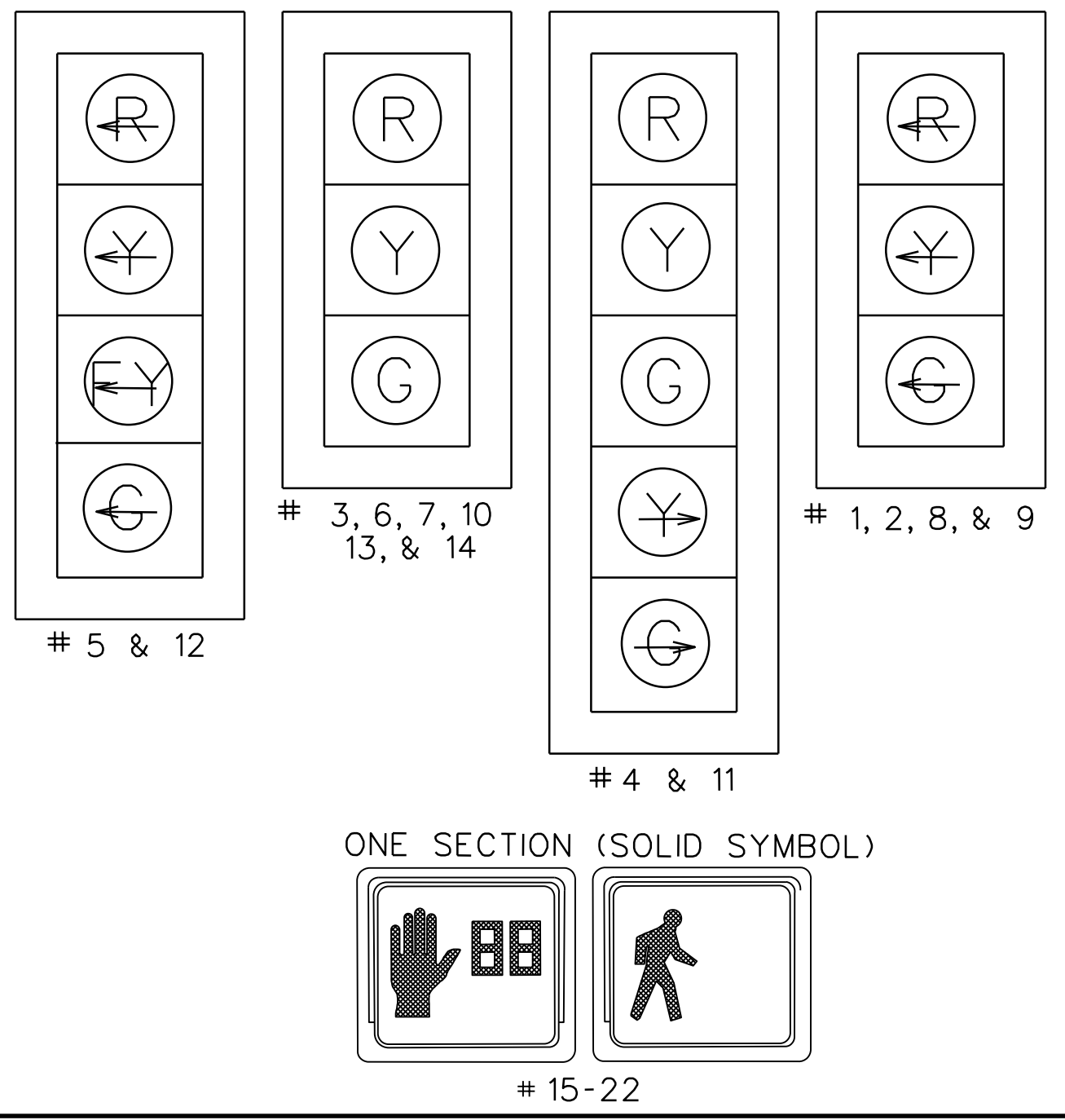
| POSTED SPEED                          | DISTANCE FROM STOP LINE |         |
|---------------------------------------|-------------------------|---------|
|                                       | LEAD VDZ                | LAG VDZ |
| 45 MPH                                | 260'                    | 115'    |
| MEDICAL CENTER PKWY. (NORTH APPROACH) |                         |         |
| VIRTUAL LOOPS                         |                         |         |
| POSTED SPEED                          | DISTANCE FROM STOP LINE |         |
|                                       | LEAD VDZ                | LAG VDZ |
| 30 MPH                                | 65'                     | N/A     |
| MEDICAL CENTER PKWY. (SOUTH APPROACH) |                         |         |
| VIRTUAL LOOPS                         |                         |         |
| POSTED SPEED                          | DISTANCE FROM STOP LINE |         |
|                                       | LEAD VDZ                | LAG VDZ |
| 25 MPH                                | 65'                     | N/A     |



PHASING DIAGRAM



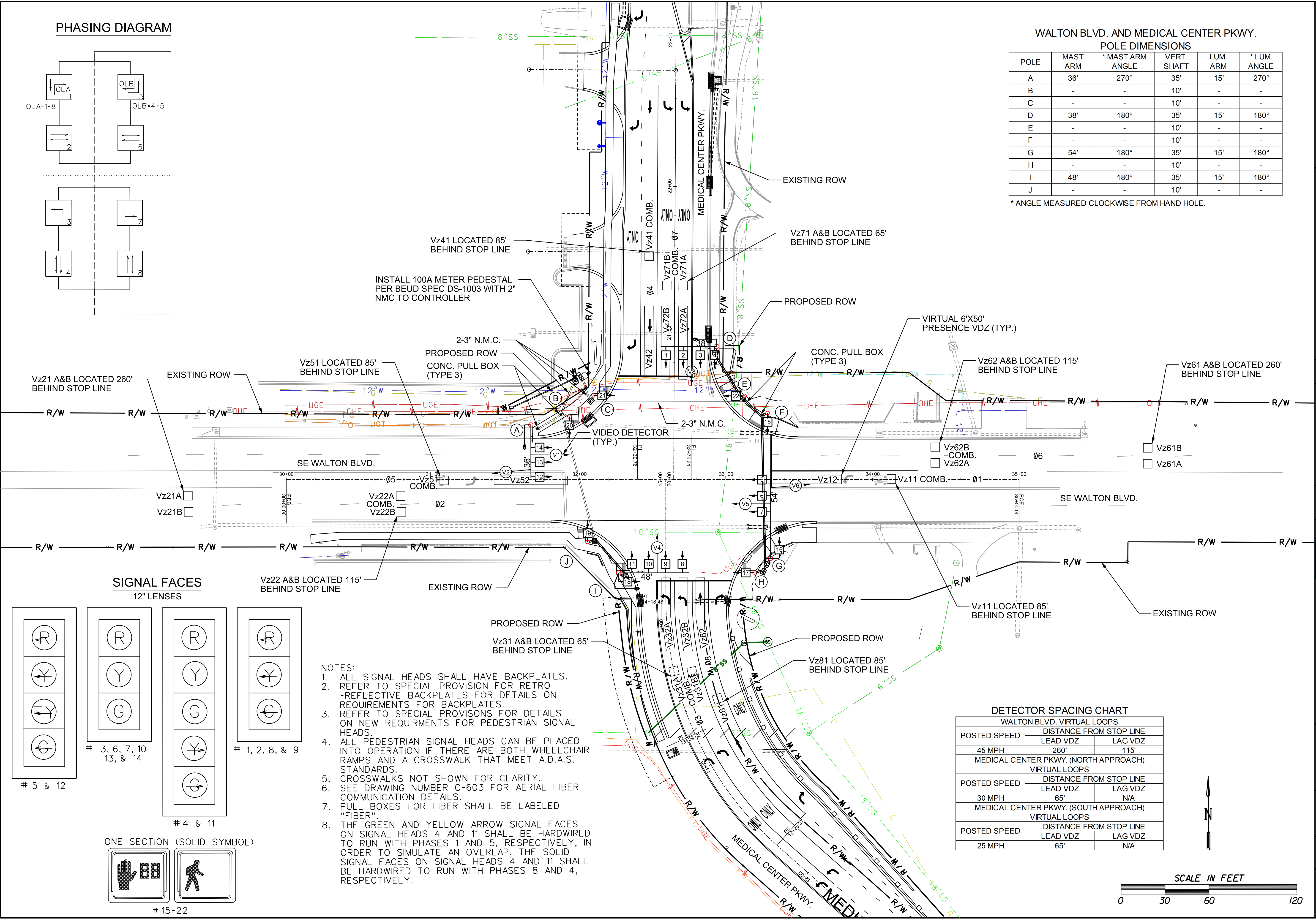
SIGNAL FACES  
 12" LENSES



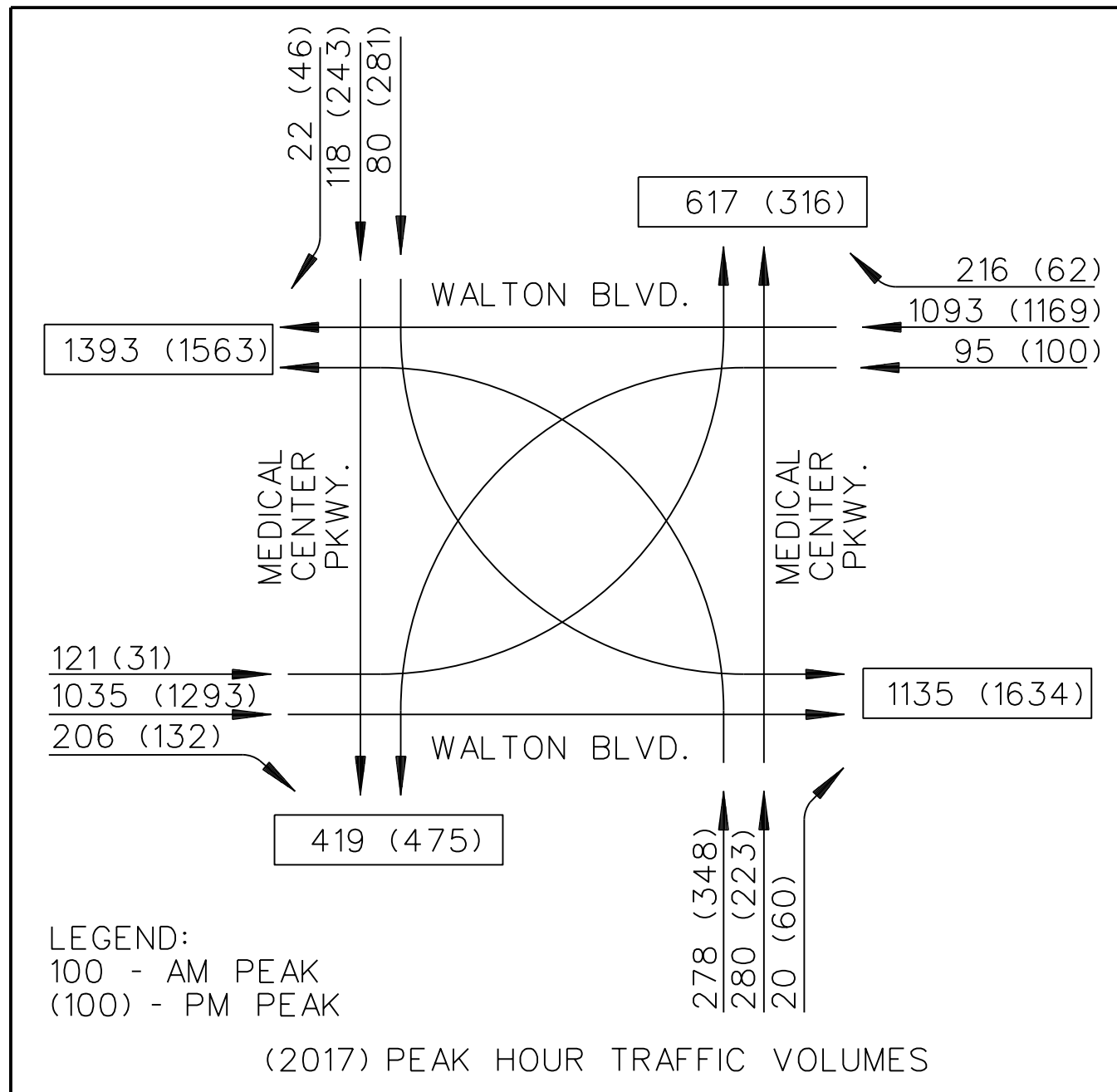
NOTES:

- ALL SIGNAL HEADS SHALL HAVE BACKPLATES.
- REFER TO SPECIAL PROVISION FOR RETRO -REFLECTIVE BACKPLATES FOR DETAILS ON REQUIREMENTS FOR BACKPLATES.
- REFER TO SPECIAL PROVISIONS FOR DETAILS ON NEW REQUIREMENTS FOR PEDESTRIAN SIGNAL HEADS.
- ALL PEDESTRIAN SIGNAL HEADS CAN BE PLACED INTO OPERATION IF THERE ARE BOTH WHEELCHAIR RAMPS AND A CROSSWALK THAT MEET A.D.A.S. STANDARDS.
- CROSSWALKS NOT SHOWN FOR CLARITY.
- SEE DRAWING NUMBER C-603 FOR AERIAL FIBER COMMUNICATION DETAILS.
- PULL BOXES FOR FIBER SHALL BE LABELED "FIBER".
- THE GREEN AND YELLOW ARROW SIGNAL FACES ON SIGNAL HEADS 4 AND 11 SHALL BE HARDWIRED TO RUN WITH PHASES 1 AND 5, RESPECTIVELY. IN ORDER TO SIMULATE AN OVERLAP, THE SOLID SIGNAL FACES ON SIGNAL HEADS 4 AND 11 SHALL BE HARDWIRED TO RUN WITH PHASES 8 AND 4, RESPECTIVELY.

TSAUSTIN 1/31/2022 12:59:32 PM  
 WORKSPACE:Garver\_2012  
 \\garver\cadd\projects\2021\21T21080 - Bentonville Walton Blvd-SE Med Ctr Imps\Drawings\WMC\_C601\_SG.dgn  
 REVISION DATE:



# TRAFFIC FLOW DIAGRAM



## DESIGN PARAMETERS

POSTED SPEED LIMIT:  
45 MPH EAST AND WEST APPROACH  
30 MPH NORTH APPROACH  
25 MPH SOUTH APPROACH

NO BUS STOPS  
NO RAILROAD TRACKS  
EXISTING INTERCONNECTIONS  
NO FIRE STATION  
NO PARKING  
NO SIGHT DISTANCE RESTRICTIONS

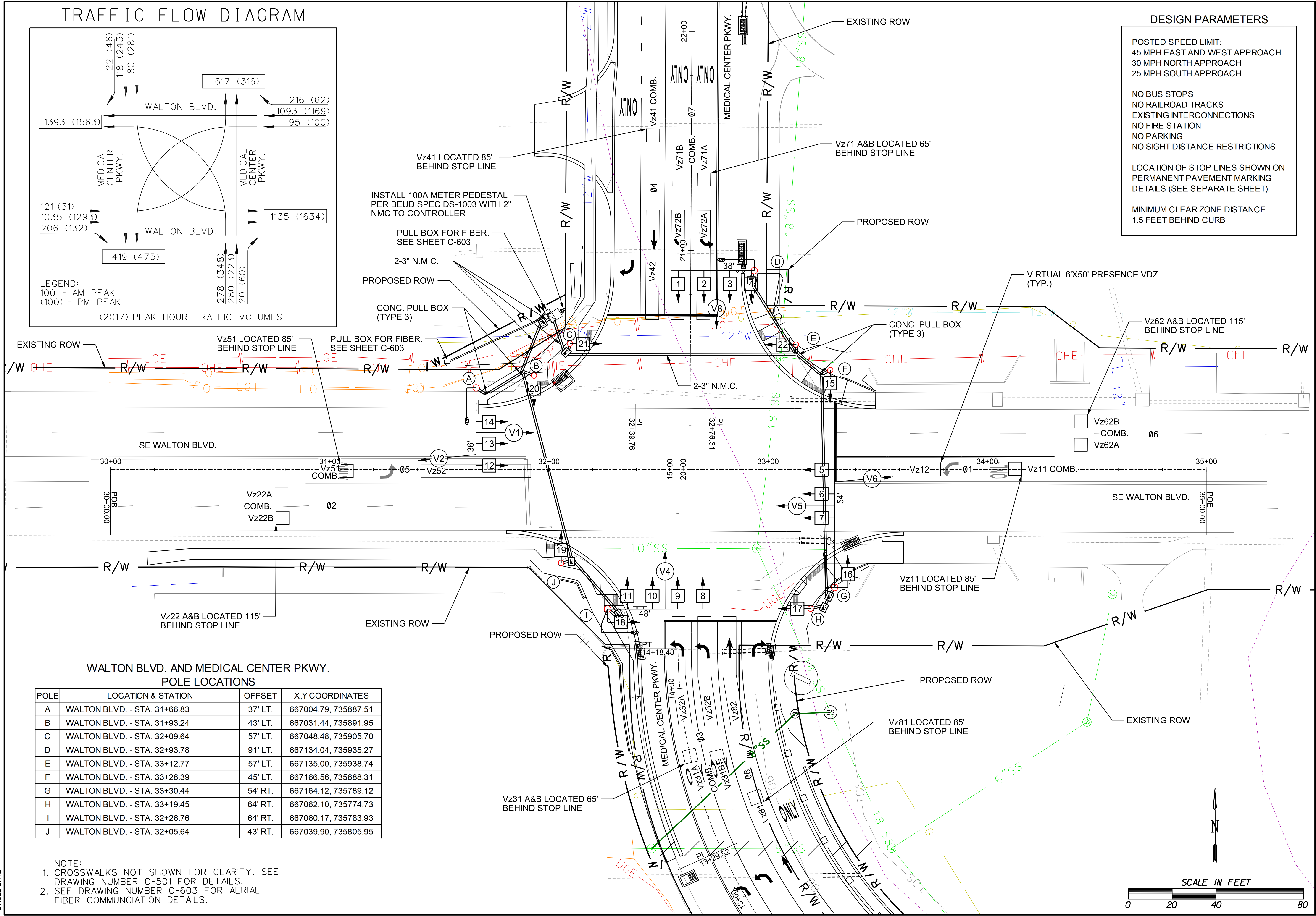
LOCATION OF STOP LINES SHOWN ON  
PERMANENT PAVEMENT MARKING  
DETAILS (SEE SEPARATE SHEET).

MINIMUM CLEAR ZONE DISTANCE  
1.5 FEET BEHIND CURB



© 2022 GARVER, LLC  
THIS DOCUMENT, ALONG WITH THE  
IDEAS AND DESIGNS CONVEYED  
HEREIN, SHALL BE CONSIDERED  
INSTRUMENTS OF PROFESSIONAL  
SERVICE AND ARE PROPERTY OF  
GARVER, LLC. ANY USE,  
REPRODUCTION, OR DISTRIBUTION  
OF THIS DOCUMENT, ALONG WITH  
THE IDEAS AND DESIGN CONTAINED  
HEREIN, IS PROHIBITED UNLESS  
AUTHORIZED IN WRITING BY  
GARVER, LLC OR EXPLICITLY  
ALLOWED IN THE GOVERNING  
PROFESSIONAL SERVICE  
AGREEMENT FOR THIS WORK.

PRELIMINARY PLANS  
FOR REVIEW ONLY  
NOT FOR CONSTRUCTION



| REV. | DATE | DESCRIPTION | BY |
|------|------|-------------|----|
|      |      |             |    |
|      |      |             |    |
|      |      |             |    |



CITY OF BENTONVILLE  
BENTONVILLE, ARKANSAS  
WALTON BOULEVARD  
AND MEDICAL CENTER  
PARKWAY IMPROVEMENTS

SIGNALIZATION  
PLANS

JOB NO.: 21T21080  
DATE: JAN. 2022  
DESIGNED BY: CEM  
DRAWN BY: HRH

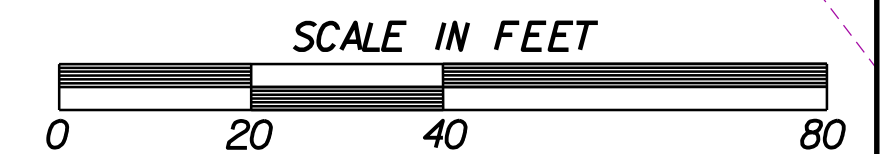
BAR IS ONE INCH ON  
ORIGINAL DRAWING  
IF NOT ONE INCH ON THIS SHEET,  
ADJUST SCALES ACCORDINGLY.

DRAWING NUMBER  
**C-602**

## WALTON BLVD. AND MEDICAL CENTER PKWY. POLE LOCATIONS

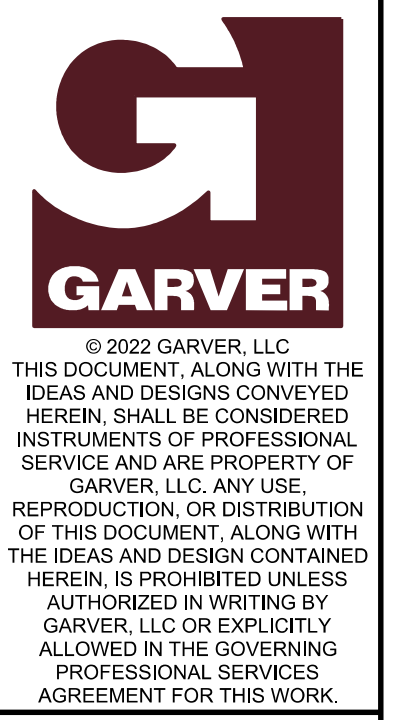
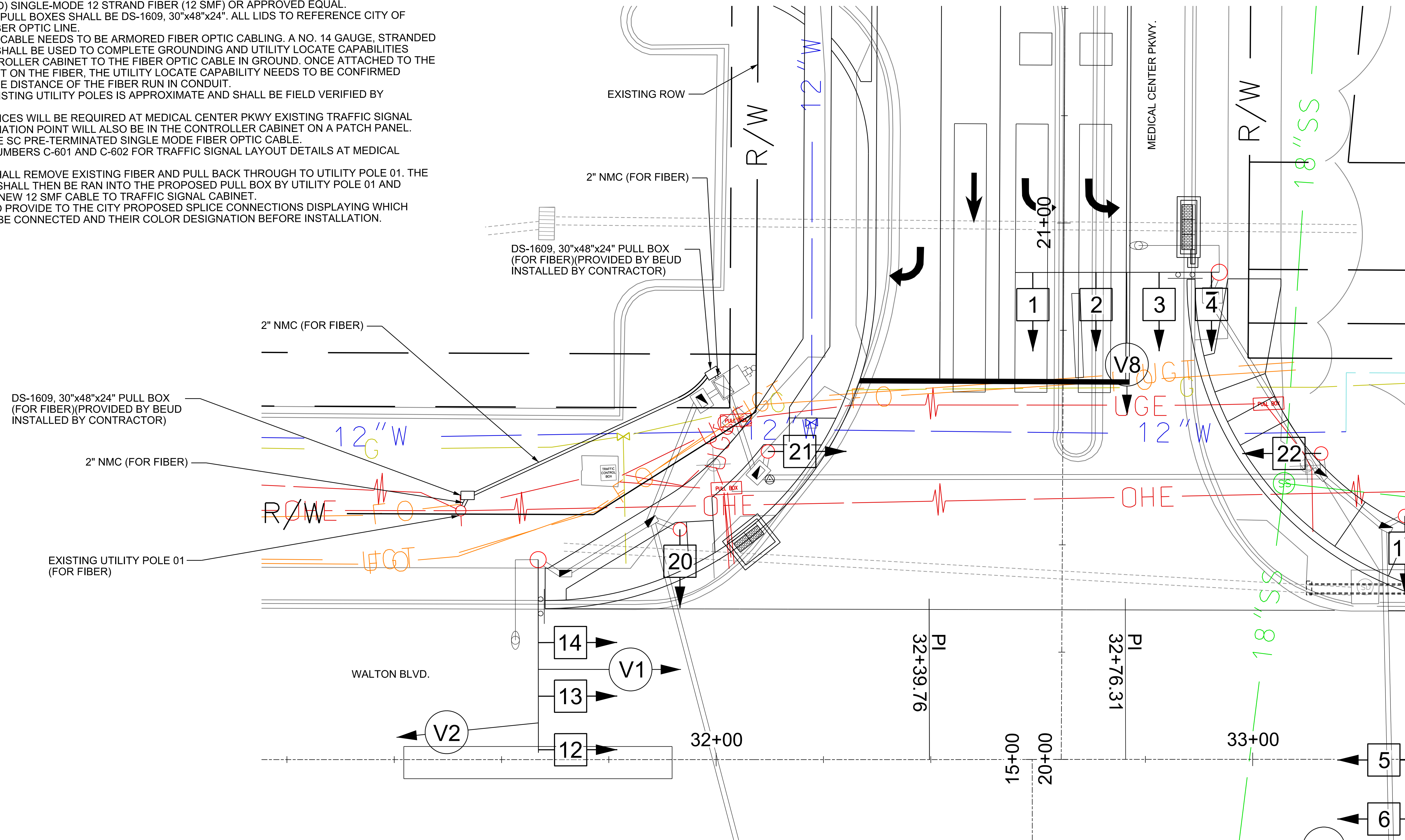
| POLE | LOCATION & STATION           | OFFSET  | X,Y COORDINATES      |
|------|------------------------------|---------|----------------------|
| A    | WALTON BLVD. - STA. 31+66.83 | 37' LT. | 667004.79, 735887.51 |
| B    | WALTON BLVD. - STA. 31+93.24 | 43' LT. | 667031.44, 735891.95 |
| C    | WALTON BLVD. - STA. 32+09.64 | 57' LT. | 667048.48, 735905.70 |
| D    | WALTON BLVD. - STA. 32+93.78 | 91' LT. | 667134.04, 735935.27 |
| E    | WALTON BLVD. - STA. 33+12.77 | 57' LT. | 667135.00, 735938.74 |
| F    | WALTON BLVD. - STA. 33+28.39 | 45' LT. | 667166.56, 735888.31 |
| G    | WALTON BLVD. - STA. 33+30.44 | 54' RT. | 667164.12, 735789.12 |
| H    | WALTON BLVD. - STA. 33+19.45 | 64' RT. | 667062.10, 735774.73 |
| I    | WALTON BLVD. - STA. 32+26.76 | 64' RT. | 667060.17, 735783.93 |
| J    | WALTON BLVD. - STA. 32+05.64 | 43' RT. | 667039.90, 735805.95 |

NOTE:  
1. CROSSWALKS NOT SHOWN FOR CLARITY. SEE  
DRAWING NUMBER C-501 FOR DETAILS.  
2. SEE DRAWING NUMBER C-603 FOR AERIAL  
FIBER COMMUNICATIONS DETAILS.



NOTES:

- THIS PROJECT INVOLVES THE MODIFICATION & INSTALLATION OF FIBER OPTIC CABLE TO INTERCONNECT THE PROPOSED TRAFFIC SIGNAL AT WALTON AND MEDICAL CENTER PKWY. TO THE EXISTING SYSTEM. ALL FIBER OPTIC WORK MUST BE COORDINATED WITH THE CITY'S TRAFFIC ENGINEERING AND IT DEPARTMENTS RESPECTFULLY BEFORE MODIFYING THE CURRENT FIBER INSTALLATION.
- ALL FIBER SHOULD BE COMMSCOPE, LIGHTSCOPE, OR CORNING ALTOS, OUTSIDE WITH MESSENGER WIRE (AS NEEDED) SINGLE-MODE 12 STRAND FIBER (12 SMF) OR APPROVED EQUAL.
- ALL FIBER OPTIC PULL BOXES SHALL BE DS-1609, 30"x48"x24". ALL LIDS TO REFERENCE CITY OF BENTONVILLE FIBER OPTIC LINE.
- ALL FIBER OPTIC CABLE NEEDS TO BE ARMORED FIBER OPTIC CABLING. A NO. 14 GAUGE, STRANDED COPPER CABLE SHALL BE USED TO COMPLETE GROUNDING AND UTILITY LOCATE CAPABILITIES FROM THE CONTROLLER CABINET TO THE FIBER OPTIC CABLE IN GROUND. ONCE ATTACHED TO THE ARMORED JACKET ON THE FIBER, THE UTILITY LOCATE CAPABILITY NEEDS TO BE CONFIRMED CAPABLE FOR THE DISTANCE OF THE FIBER RUN IN CONDUIT.
- LOCATION OF EXISTING UTILITY POLES IS APPROXIMATE AND SHALL BE FIELD VERIFIED BY CONTRACTOR.
- FIBER OPTIC SPLICES WILL BE REQUIRED AT MEDICAL CENTER PKWY EXISTING TRAFFIC SIGNAL CABINET. TERMINATION POINT WILL ALSO BE IN THE CONTROLLER CABINET ON A PATCH PANEL. PIGTAIL SHALL BE SC PRE-TERMINATED SINGLE MODE FIBER OPTIC CABLE.
- SEE DRAWING NUMBERS C-601 AND C-602 FOR TRAFFIC SIGNAL LAYOUT DETAILS AT MEDICAL CENTER PKWY.
- CONTRACTOR SHALL REMOVE EXISTING FIBER AND PULL BACK THROUGH TO UTILITY POLE 01. THE EXISTING FIBER SHALL THEN BE RAN INTO THE PROPOSED PULL BOX BY UTILITY POLE 01 AND SPLICED WITH A NEW 12 SMF CABLE TO TRAFFIC SIGNAL CABINET.
- CONTRACTOR TO PROVIDE TO THE CITY PROPOSED SPLICE CONNECTIONS DISPLAYING WHICH CABLES ARE TO BE CONNECTED AND THEIR COLOR DESIGNATION BEFORE INSTALLATION.



PRELIMINARY PLANS  
FOR REVIEW ONLY  
NOT FOR CONSTRUCTION

| REV. | DATE | DESCRIPTION | BY |
|------|------|-------------|----|
|      |      |             |    |
|      |      |             |    |
|      |      |             |    |



CITY OF BENTONVILLE  
BENTONVILLE, ARKANSAS  
WALTON BOULEVARD  
AND MEDICAL CENTER  
PARKWAY IMPROVEMENTS

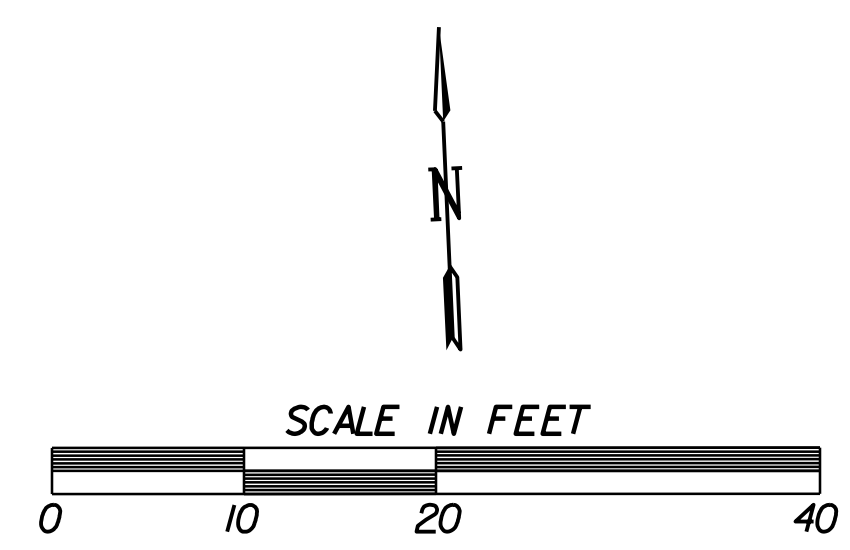
SIGNALIZATION  
PLANS

JOB NO.: 21T21080  
DATE: JAN. 2022  
DESIGNED BY: CEM  
DRAWN BY: JRM

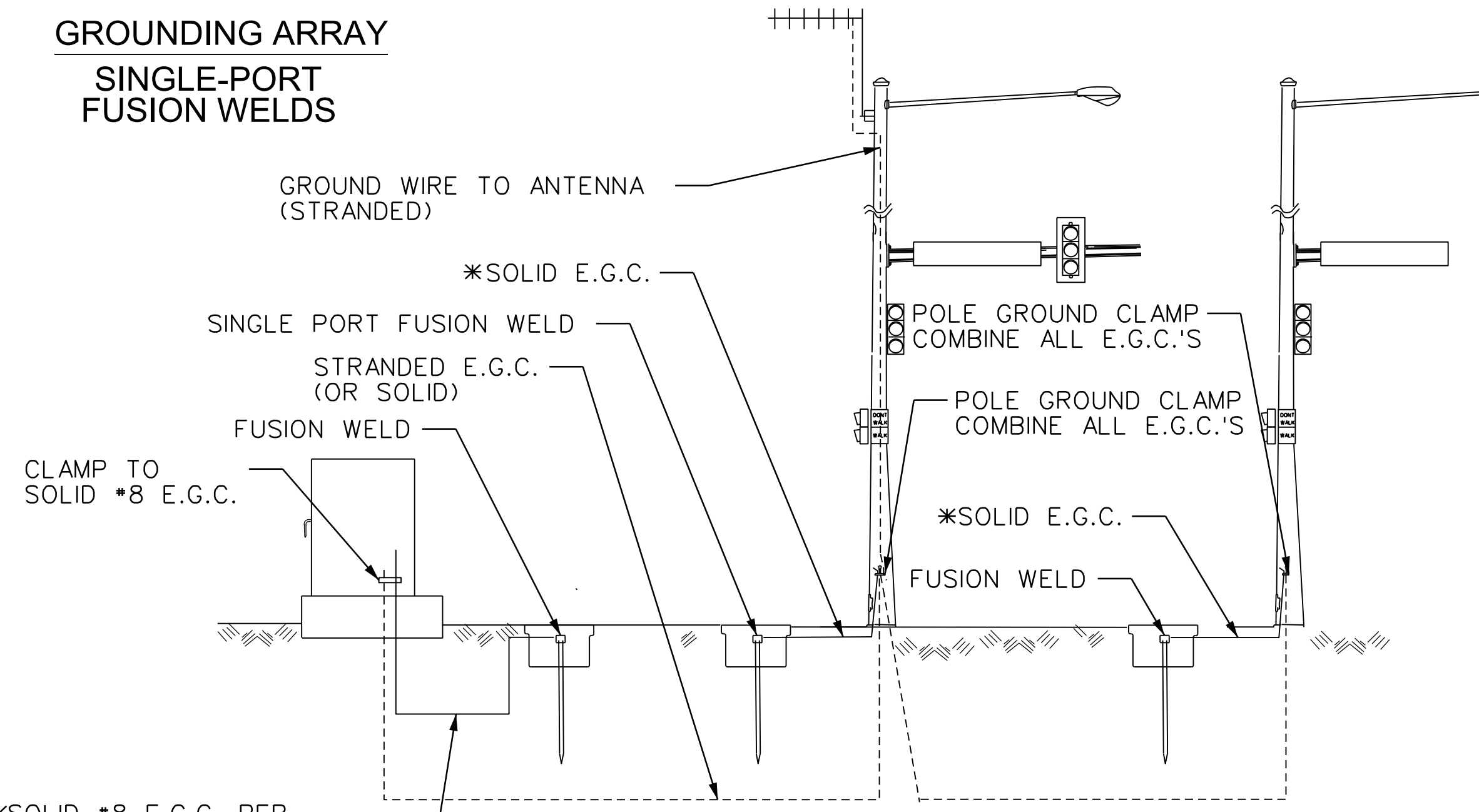
BAR IS ONE INCH ON ORIGINAL DRAWING  
IF NOT ONE INCH ON THIS SHEET, ADJUST SCALES ACCORDINGLY.

DRAWING NUMBER  
**C-603**

TSAUSTIN 1/31/2022 12:58:41 PM  
WORKSPACE: Garver\_2012  
C:\Users\tsaustin\OneDrive\Documents\Projects\2021\21T21080 - Bentonville Walton Blvd-SE Med Ctr Imps\Drawings\WMC\_C603\_SC.dgn  
REVISION DATE:



**GROUNDING ARRAY**  
**SINGLE-PORT FUSION WELDS**

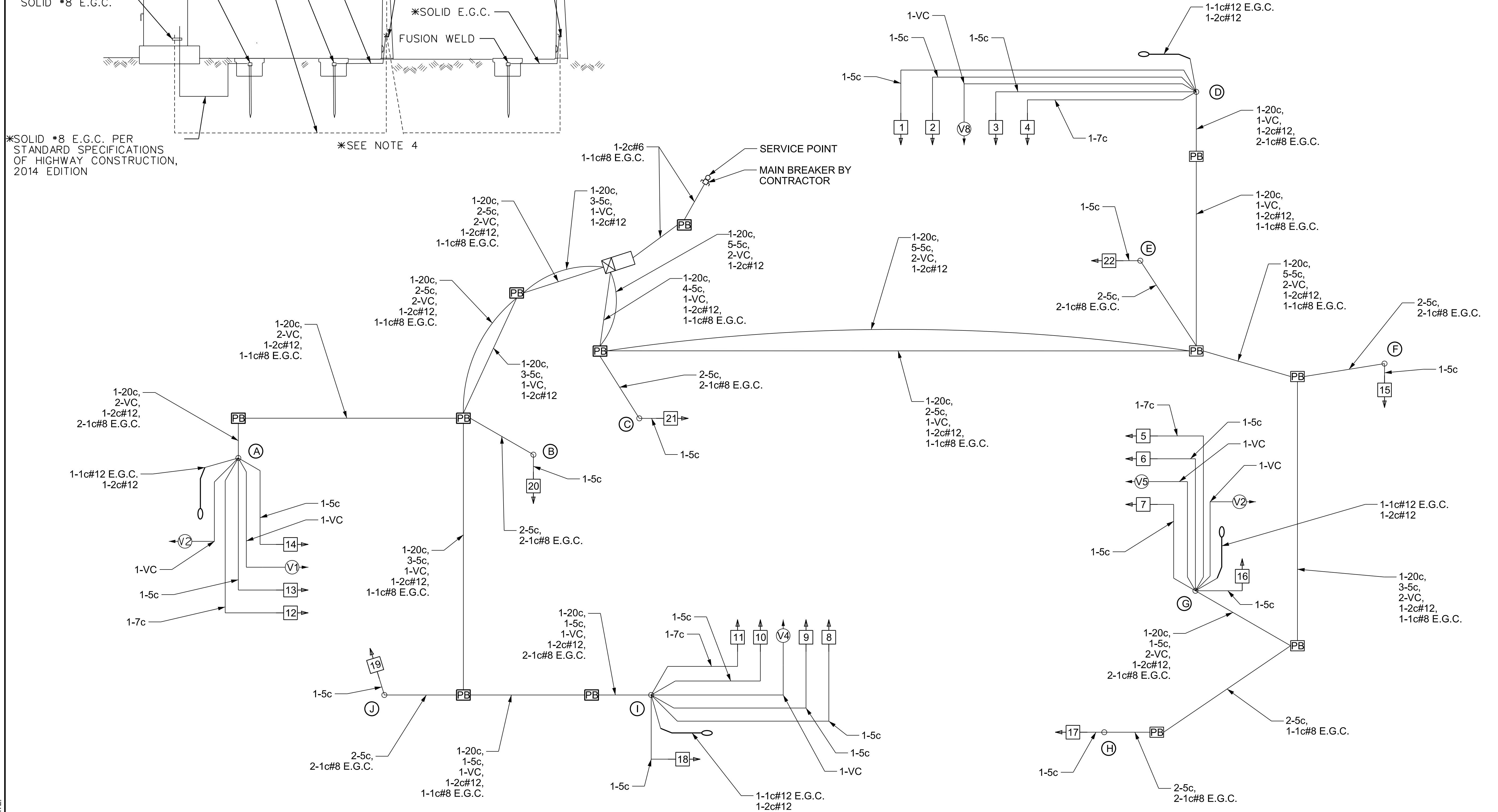


**NOTES TO CONTRACTOR:**

1. SEPARATE 5c/\*14 A.W.G. TO EACH POLE WITH PEDESTRIAN PUSH BUTTONS.
2. ALL DETECTOR RACK CHANNELS, INCLUDING UNUSED, SHALL BE BROUGHT TO TERMINAL STRIP IN DETECTOR AREA OF CABINET.
3. THE LOCAL GOVERNMENT SHALL BE RESPONSIBLE FOR PROVIDING POWER TO THE SERVICE POINT.

\*SOLID #8 E.G.C. PER STANDARD SPECIFICATIONS OF HIGHWAY CONSTRUCTION, 2014 EDITION

**WIRING DIAGRAM**  
(NOT TO SCALE)



© 2022 GARVER, LLC  
THIS DOCUMENT, ALONG WITH THE IDEAS AND DESIGNS CONVEYED HEREIN, SHALL BE CONSIDERED INSTRUMENTS OF PROFESSIONAL SERVICE AND ARE PROPERTY OF GARVER, LLC. ANY USE, REPRODUCTION, OR DISTRIBUTION OF THIS DOCUMENT, ALONG WITH THE IDEAS AND DESIGN CONTAINED HEREIN, IS PROHIBITED UNLESS AUTHORIZED IN WRITING BY GARVER, LLC OR EXPLICITLY ALLOWED IN THE GOVERNING PROFESSIONAL SERVICE AGREEMENT FOR THIS WORK.

**PRELIMINARY PLANS**  
**FOR REVIEW ONLY**  
**NOT FOR CONSTRUCTION**

| REV. | DATE | DESCRIPTION | BY |
|------|------|-------------|----|
|      |      |             |    |
|      |      |             |    |
|      |      |             |    |



**CITY OF BENTONVILLE**  
BENTONVILLE, ARKANSAS  
**WALTON BOULEVARD AND MEDICAL CENTER PARKWAY IMPROVEMENTS**

**SIGNALIZATION PLANS**

JOB NO.: 21T21080  
DATE: JAN. 2022  
DESIGNED BY: CEM  
DRAWN BY: HRH

BAR IS ONE INCH ON ORIGINAL DRAWING  
IF NOT ONE INCH ON THIS SHEET, ADJUST SCALES ACCORDINGLY.

**DRAWING NUMBER**  
**C-604**

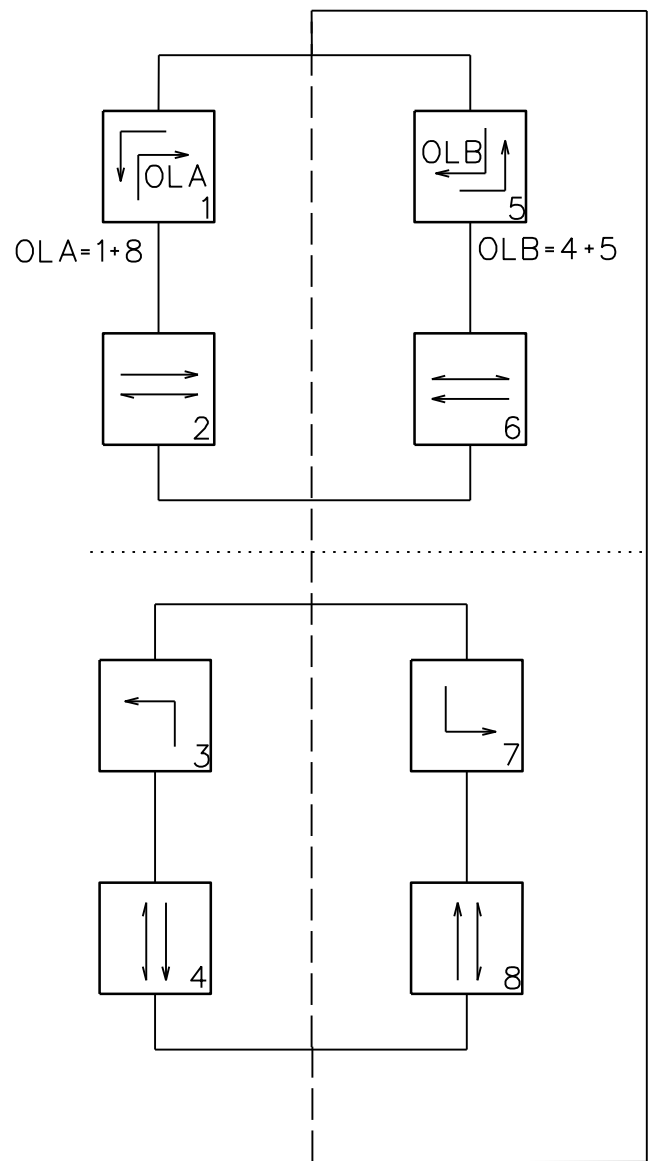
TSAUSTIN 1/31/2022 12:58:43 PM WORKSPACE:Garver\_2012 Bentley ProjectWise\Projects\2021\21T21080 - Bentonville Walton Blvd-SE Med Ctr Imps\Drawings\WMC\_C604\_SG.dgn REVISION DATE:



© 2022 GARVER, LLC  
THIS DOCUMENT, ALONG WITH THE IDEAS AND DESIGNS CONVEYED HEREIN, SHALL BE CONSIDERED INSTRUMENTS OF PROFESSIONAL SERVICE AND ARE PROPERTY OF GARVER, LLC. ANY USE, REPRODUCTION, OR DISTRIBUTION OF THIS DOCUMENT, ALONG WITH THE IDEAS AND DESIGNS CONTAINED HEREIN, IS PROHIBITED UNLESS AUTHORIZED IN WRITING BY GARVER, LLC OR EXPLICITLY ALLOWED IN THE GOVERNING PROFESSIONAL SERVICES AGREEMENT FOR THIS WORK.

PRELIMINARY PLANS FOR REVIEW ONLY NOT FOR CONSTRUCTION

**PHASING DIAGRAM**



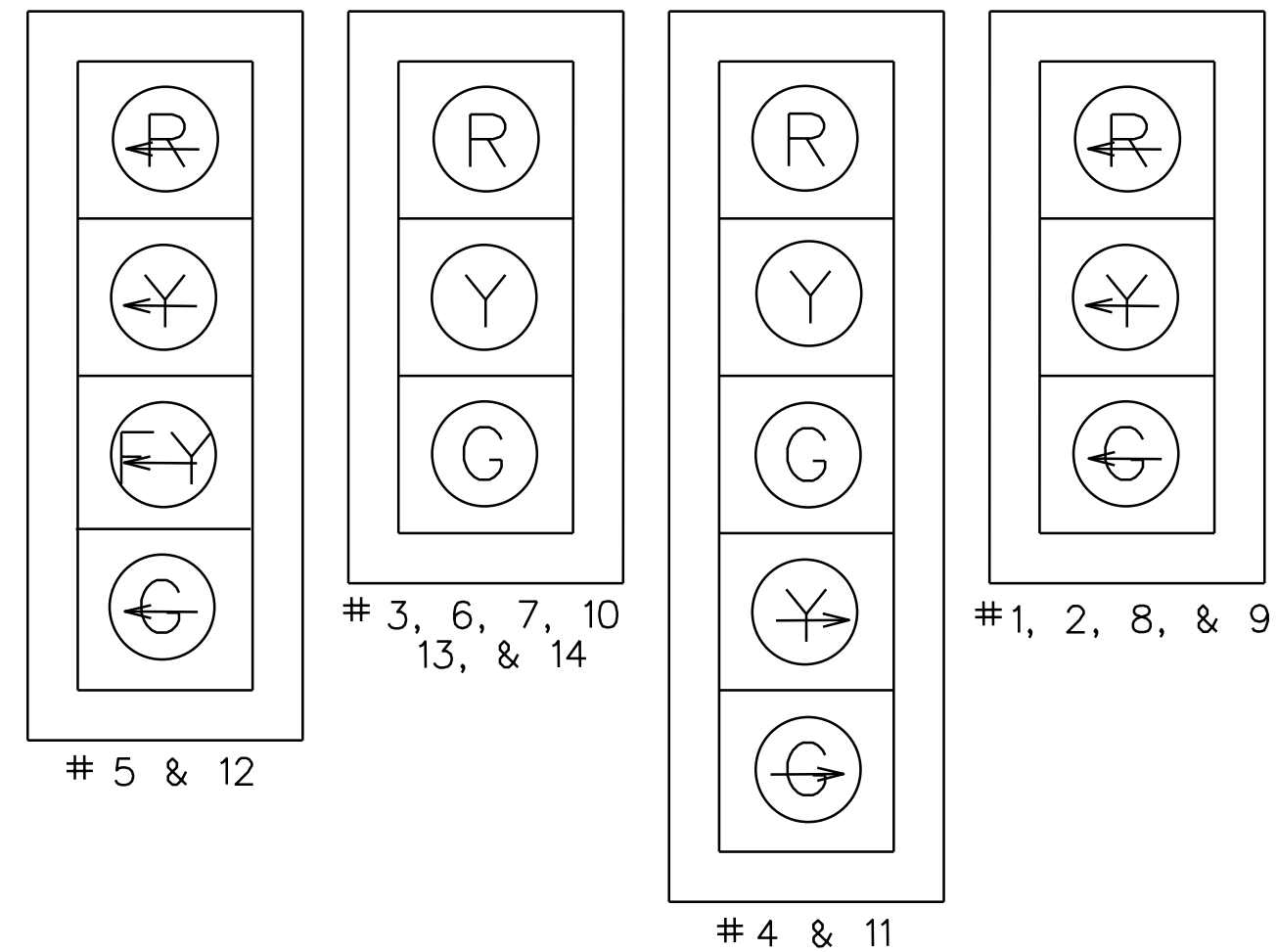
**INTERVAL CHART**

| SIGNAL FACES | WALTON BLVD. AND MEDICAL CENTER PKWY. |      |     |      |     |      |     |      |     |      |     |      |     |      |     |      | FLASH SEQUENCE |
|--------------|---------------------------------------|------|-----|------|-----|------|-----|------|-----|------|-----|------|-----|------|-----|------|----------------|
|              | 1+5                                   | CLR. | 1+6 | CLR. | 2+5 | CLR. | 2+6 | CLR. | 3+7 | CLR. | 3+8 | CLR. | 4+7 | CLR. | 4+8 | CLR. |                |
| 1 & 2        | <R                                    | <R   | <R  | <R   | <R  | <R   | <R  | <R   | <G  | *    | <G  | *    | <R  | <R   | <R  | <R   | <R             |
| 3            | R                                     | R    | R   | R    | R   | R    | R   | R    | R   | R    | G   | **   | R   | R    | G   | **   | R              |
| 4            | R                                     | R    | R   | R    | R   | R    | R   | R    | R   | R    | G   | **   | R   | R    | G   | **   | R              |
| 5            | <G                                    | *    | <FY | ***  | <G  | *    | <FY | ***  | <R  | <R   | <R  | <R   | <R  | <R   | <R  | <R   | <R             |
| 6 & 7        | R                                     | R    | R   | R    | G   | **   | G   | **   | R   | R    | R   | R    | R   | R    | R   | R    | R              |
| 8 & 9        | <R                                    | <R   | <R  | <R   | <R  | <R   | <R  | <R   | <G  | *    | <R  | <R   | <G  | *    | <R  | <R   | <R             |
| 10           | R                                     | R    | R   | R    | R   | R    | R   | R    | R   | R    | R   | R    | G   | **   | G   | **   | R              |
| 11           | R                                     | R    | R   | R    | R   | R    | R   | R    | R   | R    | R   | R    | G   | **   | G   | **   | R              |
| 12           | <G                                    | *    | <G  | *    | <FY | ***  | <FY | ***  | <R  | <R   | <R  | <R   | <R  | <R   | <R  | <R   | <R             |
| 13 & 14      | R                                     | R    | G   | **   | R   | R    | G   | **   | R   | R    | R   | R    | R   | R    | R   | R    | R              |
| 15 & 16      | DW                                    | DW   | DW  | DW   | DW  | DW   | DW  | DW   | DW  | DW   | W   | FDW  | DW  | DW   | W   | FDW  | BLK            |
| 17 & 18      | DW                                    | DW   | DW  | DW   | W   | FDW  | W   | FDW  | DW  | DW   | DW  | DW   | DW  | DW   | DW  | DW   | BLK            |
| 19 & 20      | DW                                    | DW   | DW  | DW   | DW  | DW   | DW  | DW   | DW  | DW   | DW  | DW   | W   | FDW  | W   | FDW  | BLK            |
| 21 & 22      | DW                                    | DW   | W   | FDW  | DW  | DW   | W   | FDW  | DW  | DW   | DW  | DW   | DW  | DW   | DW  | DW   | BLK            |

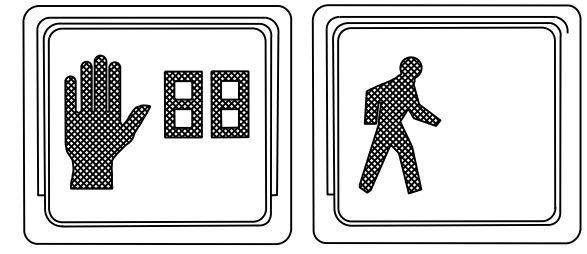
\* DENOTES GREEN OR YELLOW ARROW DEPENDING ON NEXT PHASE  
 \*\* DENOTES GREEN OR YELLOW BALL DEPENDING ON NEXT PHASE  
 \*\*\* DENOTES YELLOW OR FLASHING YELLOW ARROW DEPENDING ON NEXT PHASE

**SIGNAL FACES**

12" LENSES



ONE SECTION (SOLID SYMBOL)



- NOTES:
- ALL SIGNAL HEADS SHALL HAVE BACKPLATES. REFER TO SPECIAL PROVISION FOR RETRO-REFLECTIVE BACKPLATES FOR DETAILS ON REQUIREMENTS FOR BACKPLATES.
  - REFER TO SPECIAL PROVISIONS FOR DETAILS ON NEW REQUIREMENTS FOR PEDESTRIAN SIGNAL HEADS.
  - ALL PEDESTRIAN SIGNAL HEADS CAN BE PLACED INTO OPERATION IF THERE ARE BOTH WHEELCHAIR RAMPS AND A CROSSWALK THAT MEET A.D.A.S. STANDARDS.
  - CROSSWALKS NOT SHOWN FOR CLARITY. SEE DRAWING NUMBER C-603 FOR AERIAL FIBER COMMUNICATION DETAILS.
  - PULL BOXES FOR FIBER SHALL BE LABELED "FIBER".
  - THE GREEN AND YELLOW ARROW SIGNAL FACES ON SIGNAL HEADS 4 AND 11 SHALL BE HARDWIRED TO RUN WITH PHASES 1 AND 5, RESPECTIVELY IN ORDER TO SIMULATE AN OVERLAP. THE SOLID SIGNAL FACES ON SIGNAL HEADS 4 AND 11 SHALL BE HARDWIRED TO RUN WITH PHASES 8 AND 4, RESPECTIVELY.

**DETECTOR CHART**

| WALTON BLVD. AND MEDICAL CENTER PKWY. DETECTOR ASSIGNMENTS |                    |       |        |                             |            |             |                     |   |                                | DETECTOR SYSTEM DESCRIPTION |               |  |  | VIDEO DET. TUBE LENGTHS | COMMENTS |
|--|--------------------|-------|--------|-----------------------------|------------|-------------|---------------------|---|--------------------------------|-----------------------------|---------------|--|--|-------------------------|----------|
| DET. ID #  | LOCATION DIRECTION | TPYE  | DET. # | HARDWARE INPUTS BY SUPPLIER |            |             | PROGRAM ASSIGNMENTS |   | MASTER SYSTEM DETECTOR NUMBERS | PHS                         | SYSTEM DET. # |  |  |                         |          |
|  |                    |       |        | CAB. TRM. #                 | AMP CHN. # | CON. IMP. # | LOCAL               |   |                                |                             |               |  |  |                         |          |
| Vz11   | WB TURN LEFT FAR   | COMB. |        |                             | 1          | V9          | 1                   | 1 |                                | 37"                         | V1            |  |  |                         |          |
| Vz12   | WB LEFT TURN       | LOCAL |        |                             | 2          | V1          | 1                   |   |                                | 37"                         | V1            |  |  |                         |          |
| Vz21 A&B   | EB ADVANCE         | LOCAL |        |                             | 5          | V2          | 2                   |   |                                | 72"                         | V2            |  |  |                         |          |
| Vz22 A&B   | EB NEAR            | COMB. |        |                             | 6          | V10         | 2                   | 2 |                                | 37"                         | V5            |  |  |                         |          |
| Vz31 A&B   | NB LEFT FAR        | COMB. |        |                             | 9          | V11         | 3                   | 3 |                                | 37"                         | V3            |  |  |                         |          |
| Vz32 A&B   | NB LEFT            | LOCAL |        |                             | 10         | V3          | 3                   |   |                                | 37"                         | V3            |  |  |                         |          |
| Vz41   | SB ADVANCE         | COMB. |        |                             | 13         | V12         | 4                   | 4 |                                | 37"                         | V7            |  |  |                         |          |
| Vz42   | SB NEAR            | LOCAL |        |                             | 14         | V4          | 4                   |   |                                | 37"                         | V7            |  |  |                         |          |
| Vz51   | EB LEFT TURN FAR   | COMB. |        |                             | 7          | V13         | 5                   | 5 |                                | 37"                         | V5            |  |  |                         |          |
| Vz52   | EB LEFT TURN       | LOCAL |        |                             | 8          | V5          | 5                   |   |                                | 37"                         | V5            |  |  |                         |          |
| Vz61 A&B   | WB ADVANCE         | LOCAL |        |                             | 3          | V6          | 6                   |   |                                | 72"                         | V6            |  |  |                         |          |
| Vz62 A&B   | WB NEAR            | COMB. |        |                             | 4          | V14         | 6                   | 6 |                                | 37"                         | V1            |  |  |                         |          |
| Vz71 A&B   | SB LEFT FAR        | COMB. |        |                             | 15         | V15         | 7                   | 7 |                                | 37"                         | V7            |  |  |                         |          |
| Vz72 A&B   | SB LEFT            | LOCAL |        |                             | 16         | V7          | 7                   |   |                                | 37"                         | V7            |  |  |                         |          |
| Vz81   | NB ADVANCE         | COMB. |        |                             | 11         | V16         | 8                   | 8 |                                | 37"                         | V3            |  |  |                         |          |
| Vz82   | NB NEAR            | LOCAL |        |                             | 12         | V8          | 8                   |   |                                | 37"                         | V3            |  |  |                         |          |
| PB2  | MEDICAL S. LEG     | PED.  |        |                             |            | P2          | 2                   |   |                                |                             |               |  |  |                         |          |
| PB4  | WALTON W. LEG      | PED.  |        |                             |            | P4          | 4                   |   |                                |                             |               |  |  |                         |          |
| PB6  | MEDICAL N. LEG     | PED.  |        |                             |            | P6          | 6                   |   |                                |                             |               |  |  |                         |          |
| PB8  | WALTON E. LEG      | PED.  |        |                             |            | P8          | 8                   |   |                                |                             |               |  |  |                         |          |

CONTROLLER INPUT ABBREVIATIONS:  
 V = VEHICLE INPUT  
 D = SYSTEM OR AUXILIARY INPUT  
 P = PEDESTRIAN INPUT

NOTE: "AMP CHN =" REFERS TO THE RACK OUTPUT POSITION.  
 THIS IS WIRED TO CONTROLLER INPUT DETECTOR NUMBER WHICH IS PROGRAMMED TO ACTUATE THE DESIGNATED PHASE.  
 EXAMPLE: V9 = SYSTEM DETECTOR 1, V10 = SYSTEM DETECTOR 2

TSAUSTIN 1/31/2022 12:59:46 PM WORKSPACE:Garver\_2012 Bentley ProjectWise\Projects\2021\21T21080 - Bentonville Walton Blvd-SE Med Ctr Imps\Drawings\WMC\_C605\_Sc.dgn REVISION DATE:

| REV. | DATE | DESCRIPTION | BY |
|------|------|-------------|----|
|      |      |             |    |



CITY OF BENTONVILLE  
 BENTONVILLE, ARKANSAS  
 WALTON BOULEVARD  
 AND MEDICAL CENTER  
 PARKWAY IMPROVEMENTS

SIGNALIZATION PLANS

JOB NO.: 21T21080  
 DATE: JAN. 2022  
 DESIGNED BY: CEM  
 DRAWN BY: HRH

BAR IS ONE INCH ON ORIGINAL DRAWING  
 IF NOT ONE INCH ON THIS SHEET, ADJUST SCALES ACCORDINGLY.

DRAWING NUMBER  
**C-605**



### LEGEND

|  |                        |  |  |
|--|------------------------|--|--|
|  | BENCHMARK              |  | EXISTING CENTERLINE                    |
|  | CONTROL POINTS         |  | EXISTING MAJOR CONTOUR                 |
|  | MAIL BOX               |  | EXISTING MINOR CONTOUR                 |
|  | SIGN                   |  | EXISTING STRUCTURE                     |
|  | GAS METER              |  | EXISTING TOB/TOS                       |
|  | SANITARY MANHOLE       |  | EXISTING GRAVEL DRIVE                  |
|  | WATER VALVE            |  | EXISTING GAS UTILITY                   |
|  | HYDRANT                |  | EXISTING SANITARY UTILITY              |
|  | WATER METER            |  | EXISTING WATER UTILITY                 |
|  | STORM DRAIN MANHOLE    |  | EXISTING FENCE                         |
|  | STORM DRAIN GRATE      |  | EXISTING STORM DRAIN                   |
|  | TELEPHONE RISER        |  | EXISTING UNDERGROUND FIBER UTILITY     |
|  | TELEPHONE JUNCTION BOX |  | EXISTING UNDERGROUND TELEPHONE UTILITY |
|  | FIBER OPTIC MANHOLE    |  | EXISTING TREE LINE                     |
|  | UTILITY POLE           |  | EXISTING UNDERGROUND ELECTRIC UTILITY  |
|  | GUY ANCHOR             |  | EXISTING OVERHEAD ELECTRIC UTILITY     |
|  | LIGHT POLE             |  | EXISTING PROPERTY LINE                 |
|  | GUY POLE               |  | EXISTING RIGHT-OF-WAY                  |
|  | DECIDUOUS TREE         |  | PROPOSED TEMP. CONST. EASEMENT         |
|  | CONIFEROUS TREE        |  | PROPOSED PERMANENT EASEMENT            |
|  | BOREHOLE               |  | PROPOSED RIGHT-OF-WAY                  |
|  |                        |  | PROPOSED CENTERLINE                    |
|  |                        |  | PROPOSED CURB AND GUTTER               |
|  |                        |  | PROPOSED ROADWAY OUTLINE               |
|  |                        |  | PROPOSED STORM DRAIN                   |
|  |                        |  | PROPOSED TOP-OF-BANK                   |
|  |                        |  | PROPOSED TOE-OF-SLOPE                  |
|  |                        |  | PROPOSED UTILITY LINE ABANDONMENT      |
|  |                        |  | PROPOSED C.L. DITCH / CHANNEL          |

**GENERAL UTILITY NOTES:**

- SAFETY SHALL BE THE SOLE RESPONSIBILITY OF THE CONTRACTOR IN STRICT ACCORDANCE WITH OSHA STANDARDS. THE OWNER AND ENGINEER SHALL NOT BE RESPONSIBLE FOR SAFETY, MEANS AND METHODS OF THE CONTRACTOR.
- CAUTION: UNDERGROUND UTILITIES MAY EXIST WITHIN AND ADJACENT TO THE LIMITS OF CONSTRUCTION. AN ATTEMPT HAS BEEN MADE TO LOCATE THESE UTILITIES ON THE PLANS. HOWEVER, ALL EXISTING UTILITIES MAY NOT BE SHOWN AND THE ACTUAL LOCATIONS OF THE UTILITIES MAY VARY FROM THE LOCATIONS SHOWN. PRIOR TO BEGINNING ANY TYPE OF EXCAVATION THE CONTRACTOR SHALL CONTACT THE UTILITIES INVOLVED AND MAKE ARRANGEMENTS FOR THE LOCATION OF THE UTILITIES ON THE GROUND. THE CONTRACTOR SHALL MAINTAIN THE UTILITY LOCATION MARKINGS UNTIL THEY ARE NO LONGER NECESSARY. ARKANSAS STATE LAW, THE UNDERGROUND FACILITIES DAMAGE PREVENTION ACT, REQUIRES TWO WORKING DAYS ADVANCE NOTIFICATION THROUGH THE ARKANSAS ONE-CALL SYSTEM CENTER BEFORE EXCAVATING USING MECHANIZED EQUIPMENT OR EXPLOSIVES (EXCEPT IN THE CASE OF EMERGENCY). THE ONE-CALL SYSTEM PHONE NUMBER IS 1-800-482-8998 OR 811. THE CONTRACTOR IS ADVISED THAT THERE IS A SEVERE PENALTY FOR NOT MAKING THIS CALL. NOT ALL UTILITY COMPANIES ARE MEMBERS OF THE ARKANSAS ONE-CALL SYSTEM. THEREFORE, THE CONTRACTOR IS ADVISED TO CONTACT ALL NON-MEMBER UTILITIES AS WELL AS THE ONE-CALL SYSTEM. THE LOCATION OF THE EXISTING UTILITIES SHOWN ON THE PLANS ARE APPROXIMATE, AND ARE THE LOCATIONS AT THE TIME OF DESIGN. SOME UTILITIES MAY HAVE BEEN RELOCATED BETWEEN THE TIME OF DESIGN AND THE CONTRACTOR'S NOTICE TO PROCEED. PRIOR TO ANY CONSTRUCTION, COORDINATE WITH UTILITY OWNER AND SPOT DIG AND VERIFY LOCATIONS WITH ENGINEER.
- ALL UTILITY WORK SHALL BE PER THE 2008 EDITION OF THE CITY OF BENTONVILLE STANDARD WATER AND SEWER SPECIFICATIONS WHICH ARE INCLUDED IN THE CONTRACT DOCUMENTS BY REFERENCE.
- CONTRACTOR SHALL NOTIFY APPLICABLE UTILITY OWNERS PRIOR TO ANY SPOT DIGGING. CONTRACTOR SHALL NOTIFY BLACK HILLS ENERGY ANYTIME WORKING NEAR GAS LINES. CONTRACTOR SHALL NOTIFY UTILITY OWNER IMMEDIATELY TO REPORT ANY DAMAGE TO EXISTING UTILITY. CONTRACTOR SHALL MAINTAIN AND PROTECT ALL EXISTING UTILITIES INCLUDING STORM SEWERS.
- CONTRACTOR SHALL PROVIDE CONSTRUCTION STAKING BY A QUALIFIED SURVEYOR LICENSED AS A PLS IN ARKANSAS. VERIFY SURVEY CONTROL PRIOR TO ANY CONSTRUCTION STAKING. PROPERTY MONUMENTS DISTURBED BY CONSTRUCTION ACTIVITIES SHALL BE REPLACED. CONTRACTOR SHALL PROVIDE SURVEY FOR VALVES, METERS, AND FIRE HYDRANTS FOR RECORD DRAWINGS.
- CONTRACTOR SHALL ENSURE THAT STORMWATER DEBRIS AND WATER DO NOT ENTER PIPE ENDS AT ALL TIMES WITH TEMPORARY CAPS. CONTRACTOR SHALL PROTECT PIPE FROM FLOATING UNTIL FINAL COVER HAS BEEN PLACED.
- ALL VALVES AND FITTINGS SHALL BE RESTRAINED WITH RESTRAINT GLANDS. ALL FITTINGS SHALL ALSO HAVE CONCRETE THRUST BLOCKING.
- PIPE BACKFILL SHALL BE COMPACTED CLASS 7 AGGREGATE BASE COURSE AT EXISTING, PROPOSED, AND FUTURE TRAFFIC LOCATIONS AS SHOWN ON PROFILES IN 8" MAXIMUM LIFTS AND COMPACTED TO 95% MODIFIED PROCTOR DENSITY. TEMPORARY AND PERMANENT PAVEMENT REPAIR SHALL BE PER DETAILS.
- CONTRACTOR SHALL SPOT DIG AND VERIFY LOCATIONS OF ALL UTILITIES AT CONNECTIONS AND INTERSECTIONS OF PROPOSED WATER AND SEWER FACILITIES PRIOR TO ANY CONSTRUCTION. PIPE ELEVATIONS ARE FOR REFERENCE ONLY. VERIFY EXISTING CONFLICTS AND COORDINATE WITH ENGINEER.
- CONTRACTOR SHALL SPOT DIG AND VERIFY EXISTING JOINTS AT PROPOSED TAPPING SLEEVE LOCATIONS PRIOR TO ANY WORK. MAINTAIN 5' MINIMUM HORIZONTAL SEPARATION BETWEEN TAPPING SLEEVE AND EXISTING PIPE JOINTS. ADJUST TAPPING LOCATIONS AS APPROVED BY ENGINEER.
- CONTRACTOR SHALL MAINTAIN ALL CONSTRUCTION ACTIVITIES WITHIN RIGHT-OF-WAY AND EASEMENTS.
- WATER LINE PIPES SHALL BE CLASS 50 DUCTILE IRON (RATED AT 350 PSI) OR AWWA C-900 DR-14 PVC AS SPECIFIED IN THE DRAWINGS, IN ACCORDANCE WITH OWNER'S REQUIREMENTS. JOINTS SHALL BE FIELD-LOK WITHIN STEEL ENCASUREMENT.
- CONTRACTOR SHALL PROVIDE GPS COORDINATES FOR ALL NEW SANITARY SEWER MANHOLES, WATER VALVES, AND HORIZONTAL WATER LINE BENDS FOR RECORD DRAWINGS.
- INSTALL LINE/VALVE MARKERS AT LOCATIONS SHOWN ON THE PLANS, OR AS DIRECTED BY THE ENGINEER OR OWNER.
- CONTINGENT UPON GROUNDWATER CONDITIONS, INSTALL WATER MITIGATION DAMS AT LOCATIONS AS DIRECTED BY ENGINEER/OWNER. MITIGATION DAMS SHALL BE FLOWABLE FILL, KEYED INTO UNDISTURBED SOIL 12", 4' ABOVE TOP OF PIPE, AND 3' MINIMUM LONGITUDINAL LENGTH.
- CONTRACTOR SHALL IMMEDIATELY HALT CONSTRUCTION ACTIVITIES AND NOTIFY OWNER WHEN ANY CAVES OR ARCHEOLOGICAL REMAINS AND OR ARTIFACTS ARE DISCOVERED. THE OWNER WILL IMMEDIATELY NOTIFY USFWS FOR CAVES AND USACE FOR ARTIFACTS.
- STORAGE AREA LOCATIONS OUTSIDE EASEMENTS SHALL BE APPROVED BY OWNER/ENGINEER.
- PAINT FIRE HYDRANT BONNET AND CAP PER FLOW RATE AS SPECIFIED BY OWNER.
- CONTRACTOR SHALL INSTALL SERVICE SADDLES AND CORP STOPS FOR PRESSURE TESTS, BLOWOFF, AND DISINFECTION AS APPROVED BY THE ENGINEER AND OWNER.
- REGARDING SANITARY SEWER BYPASS PUMPING:
  - NO PUMPING WILL BE ALLOWED OVERNIGHT. THE GRAVITY SEWER SYSTEM SHALL BE OPERATIONAL AT THE END OF EACH WORKING DAY.
  - A REDUNDANT PUMP OF EQUAL OR GREATER CAPACITY SHALL BE AVAILABLE ON-SITE.
  - PUMPING OPERATIONS SHALL NOT BE PERFORMED WHEN THERE IS A 20 PERCENT OR GREATER CHANCE OF RAIN.
  - THE CONTRACTOR SHALL SUCCESSFULLY DEMONSTRATE THE SYSTEM'S ABILITY TO PUMP FOR FOUR (4) HOURS BEFORE BYPASS PUMPING WILL BE ALLOWED TO BEGIN.
  - THE CONTRACTOR SHALL SUPPLY SUBMITTALS AND BYPASS PUMPING PLANS FOR APPROVAL BEFORE BYPASS PUMPING WILL BE ALLOWED.
  - ADEQUATELY-TRAINED PERSONNEL SHALL BE ON-SITE AT ALL TIMES DURING SETUP, OPERATION, AND BREAK-DOWN OF BYPASS PUMPING.

© 2022 GARVER, LLC  
 THIS DOCUMENT, ALONG WITH THE IDEAS AND DESIGNS CONVEYED HEREIN, SHALL BE CONSIDERED INSTRUMENTS OF PROFESSIONAL SERVICE AND ARE PROPERTY OF GARVER, LLC. ANY USE, REPRODUCTION, OR DISTRIBUTION OF THIS DOCUMENT, ALONG WITH THE IDEAS AND DESIGN CONTAINED HEREIN, IS PROHIBITED UNLESS AUTHORIZED IN WRITING BY GARVER, LLC OR EXPLICITLY ALLOWED IN THE GOVERNING PROFESSIONAL SERVICES AGREEMENT FOR THIS WORK.

**PRELIMINARY PLANS  
 FOR REVIEW ONLY  
 NOT FOR CONSTRUCTION**

| REV. | DATE | DESCRIPTION | BY |
|------|------|-------------|----|
|      |      |             |    |
|      |      |             |    |
|      |      |             |    |



**CITY OF BENTONVILLE**  
 BENTONVILLE, ARKANSAS  
**WALTON BOULEVARD  
 AND MEDICAL CENTER  
 PARKWAY IMPROVEMENTS**

WATERLINE AND SEWERLINE GENERAL NOTES

JOB NO.: 21T21080  
 DATE: JAN. 2022  
 DESIGNED BY: LFG  
 DRAWN BY: TWM

BAR IS ONE INCH ON ORIGINAL DRAWING  
 0 1" IF NOT ONE INCH ON THIS SHEET, ADJUST SCALES ACCORDINGLY.

DRAWING NUMBER  
**C-900**

**ARKANSAS ONE CALL**



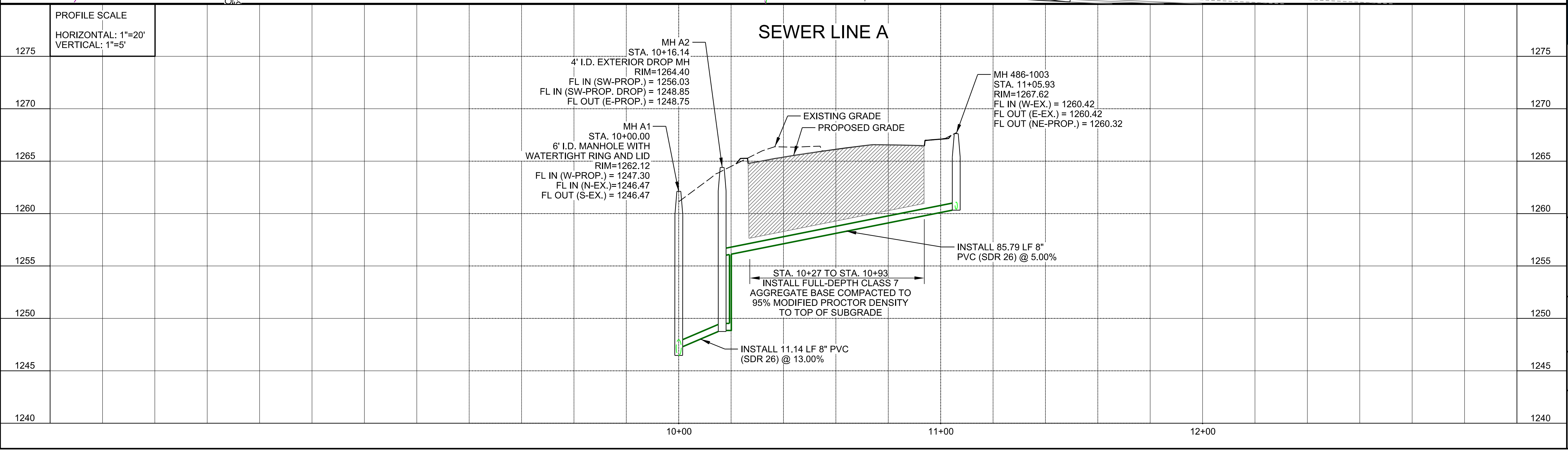
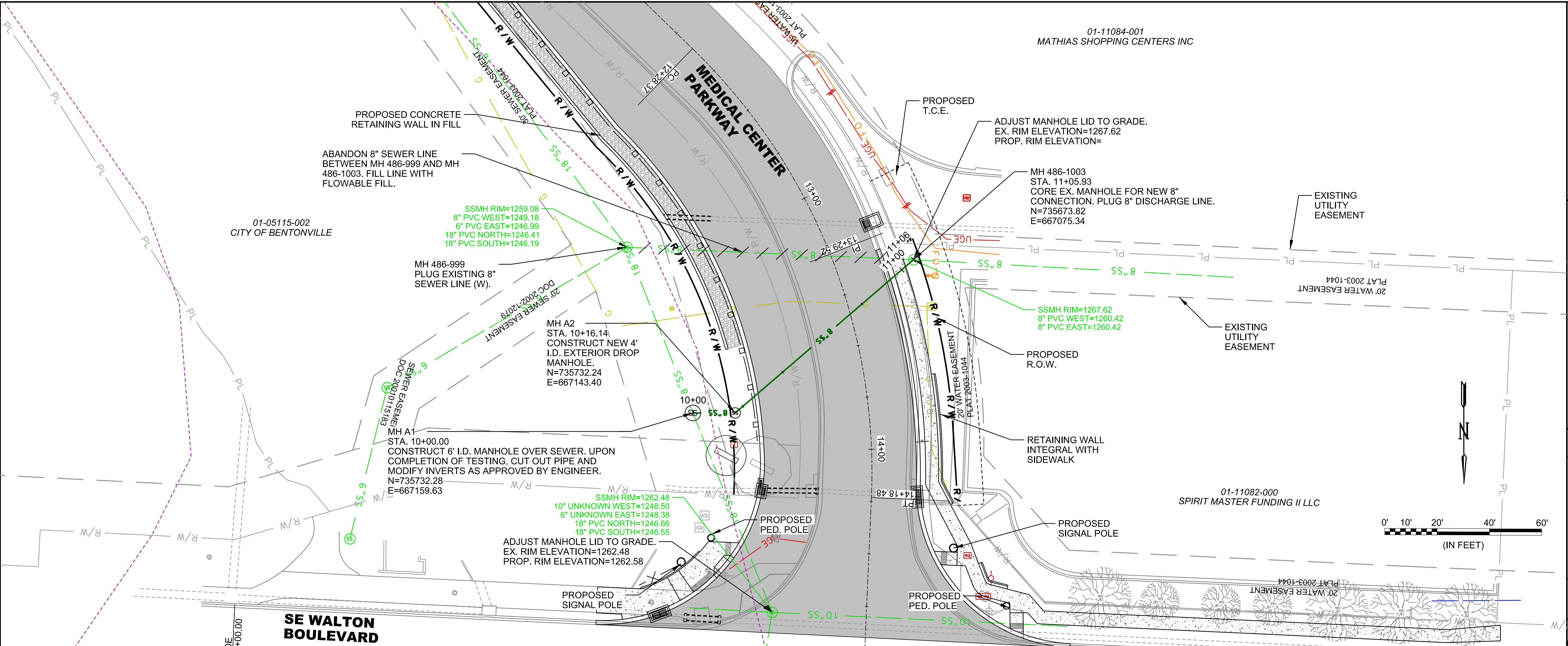
**Know what's below.  
 Call before you dig.**  
 SEE GENERAL UTILITY NOTE 2



© 2022 GARVER, LLC  
 THIS DOCUMENT, ALONG WITH THE IDEAS AND DESIGNS CONVEYED HEREIN, SHALL BE CONSIDERED INSTRUMENTS OF PROFESSIONAL SERVICE AND ARE PROPERTY OF GARVER, LLC. ANY USE, REPRODUCTION, OR DISTRIBUTION OF THIS DOCUMENT, ALONG WITH THE IDEAS AND DESIGN CONTAINED HEREIN, IS PROHIBITED UNLESS AUTHORIZED IN WRITING BY GARVER, LLC OR EXPLICITLY ALLOWED IN THE GOVERNING PROFESSIONAL SERVICES AGREEMENT FOR THIS WORK.

**PRELIMINARY PLANS  
 FOR REVIEW ONLY  
 NOT FOR CONSTRUCTION**

| REV. | DATE | DESCRIPTION | BY |
|------|------|-------------|----|
|      |      |             |    |
|      |      |             |    |
|      |      |             |    |



**CITY OF BENTONVILLE**  
 BENTONVILLE, ARKANSAS

**WALTON BOULEVARD AND MEDICAL CENTER PARKWAY IMPROVEMENTS**

(S) MEDICAL CENTER PARKWAY SEWER LINE A PLAN AND PROFILE

JOB NO.: 21T21080  
 DATE: JAN. 2022  
 DESIGNED BY: LFG  
 DRAWN BY: TWM

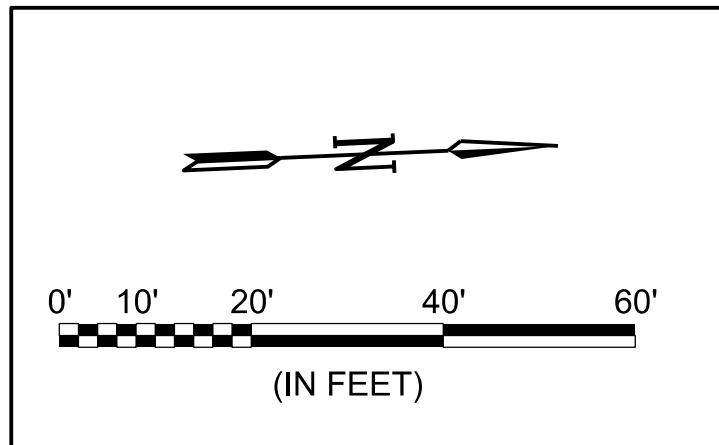
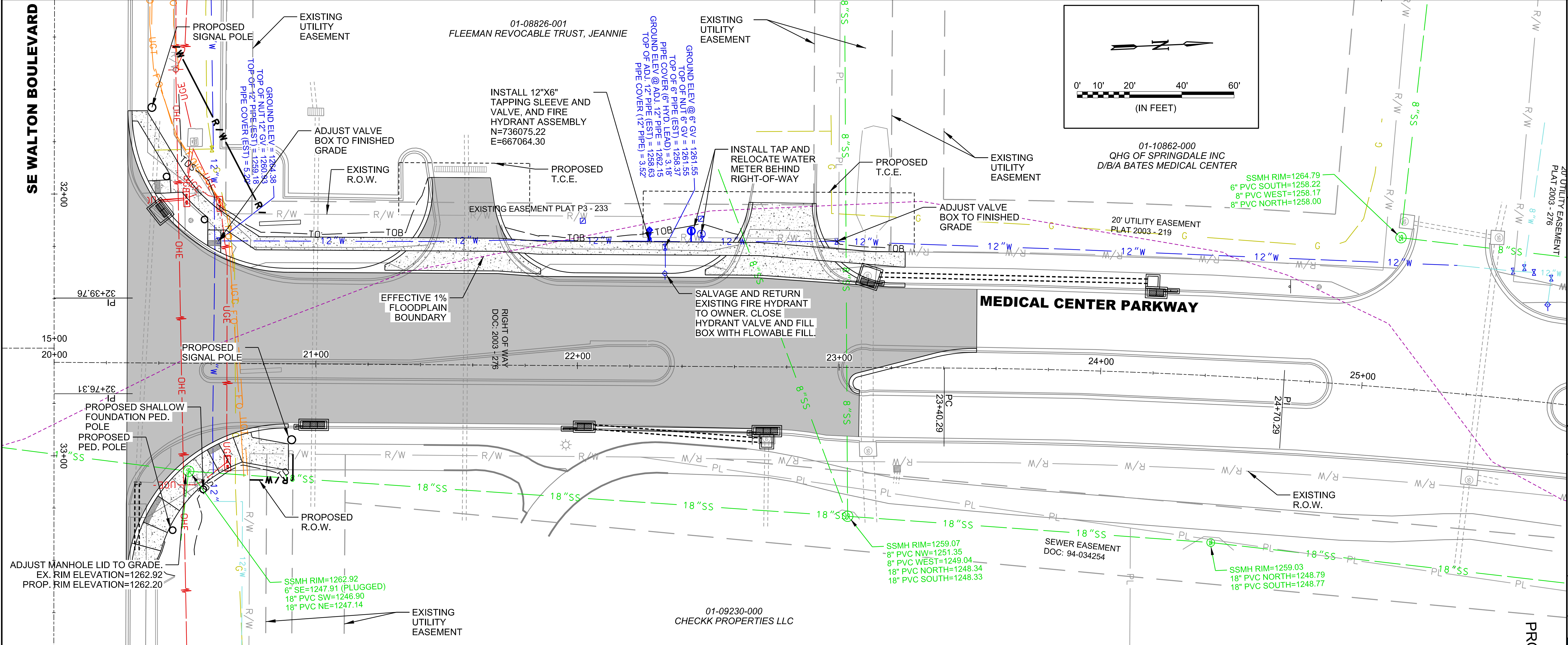
BAR IS ONE INCH ON ORIGINAL DRAWING  
 IF NOT ONE INCH ON THIS SHEET, ADJUST SCALES ACCORDINGLY.

DRAWING NUMBER  
**C-901**

TSAUSTIN 1/26/2022 4:08:37 PM  
 WORKSPACE: Garver\_2012  
 PROJECT: Bentonville Parkway Improvements  
 REVISION DATE:

1/28/2022 8:41:52 AM  
 WORKSPACE:Garver\_2012  
 J:\Adams\Projects\Bentonville\Waltton Blvd-SE - Bentonville - Bentonville\Drawings\WMC\_C902\_WP.dgn  
 REVISION DATE:

SE WALTON BOULEVARD



**GARVER**

© 2022 GARVER, LLC  
 THIS DOCUMENT, ALONG WITH THE IDEAS AND DESIGNS CONVEYED HEREIN, SHALL BE CONSIDERED INSTRUMENTS OF PROFESSIONAL SERVICE AND ARE PROPERTY OF GARVER, LLC. ANY USE, REPRODUCTION, OR DISTRIBUTION OF THIS DOCUMENT, ALONG WITH THE IDEAS AND DESIGNS CONTAINED HEREIN, IS PROHIBITED UNLESS AUTHORIZED IN WRITING BY GARVER, LLC OR EXPLICITLY ALLOWED IN THE GOVERNING PROFESSIONAL SERVICE AGREEMENT FOR THIS WORK.

**PRELIMINARY PLANS  
 FOR REVIEW ONLY  
 NOT FOR CONSTRUCTION**

| REV. | DATE | DESCRIPTION | BY |
|------|------|-------------|----|
|      |      |             |    |
|      |      |             |    |
|      |      |             |    |

|                              |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |
|------------------------------|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|
| <b>NO PROFILE THIS SHEET</b> |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |
|------------------------------|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|

**CITY OF BENTONVILLE**  
 BENTONVILLE, ARKANSAS

**WALTON BOULEVARD  
 AND MEDICAL CENTER  
 PARKWAY IMPROVEMENTS**

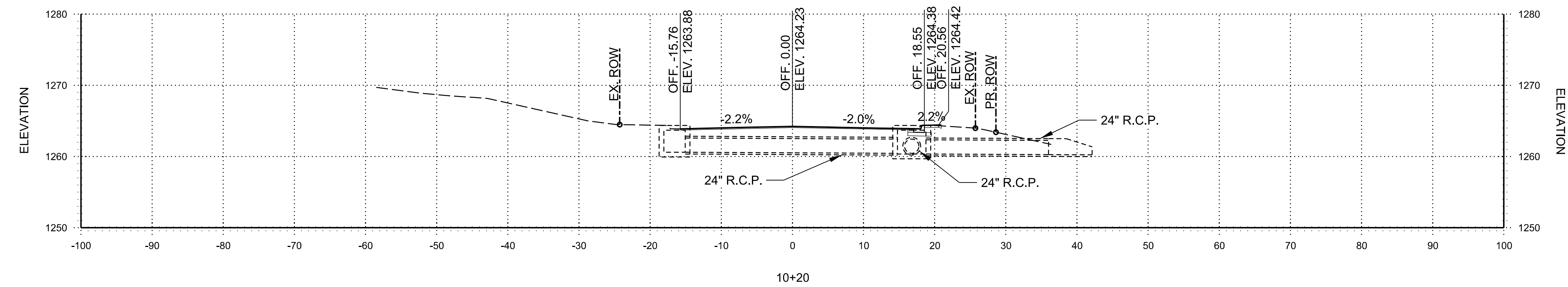
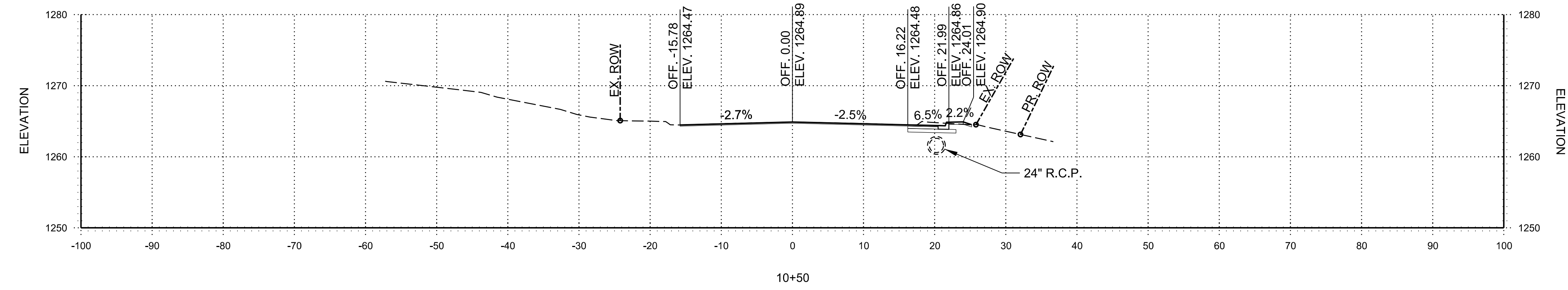
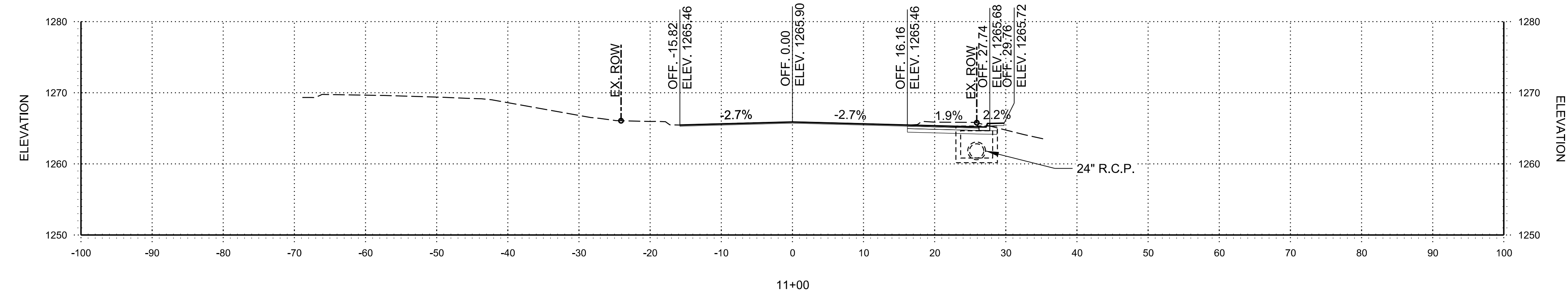
(N) MEDICAL CENTER PARKWAY WATER AND SEWER ADJUSTMENTS

JOB NO.: 21T21080  
 DATE: JAN. 2022  
 DESIGNED BY: LFG  
 DRAWN BY: TWMM

BAR IS ONE INCH ON ORIGINAL DRAWING  
 0" = 1" (INCHES)  
 IF NOT ONE INCH ON THIS SHEET, ADJUST SCALES ACCORDINGLY.

DRAWING NUMBER  
**C-902**

TSAUSTIN 1/26/2022 4:08:42 PM  
 WORKSPACE:Garver\_2012  
 Reviewing Data:Projects\2021\21T21080 - Bentonville Walton Blvd-SE Med Ctr Imps\Drawings\WMC\_X101\_CX.dgn  
 REVISION DATE:



STA. 10+20 TO STA. 11+00



© 2022 GARVER, LLC  
 THIS DOCUMENT, ALONG WITH THE  
 IDEAS AND DESIGNS CONVEYED  
 HEREIN, SHALL BE CONSIDERED  
 INSTRUMENTS OF PROFESSIONAL  
 SERVICE AND ARE PROPERTY OF  
 GARVER, LLC. ANY USE,  
 REPRODUCTION, OR DISTRIBUTION  
 OF THIS DOCUMENT, ALONG WITH  
 THE IDEAS AND DESIGN CONTAINED  
 HEREIN, IS PROHIBITED UNLESS  
 AUTHORIZED IN WRITING BY  
 GARVER, LLC OR EXPLICITLY  
 ALLOWED IN THE GOVERNING  
 PROFESSIONAL SERVICE  
 AGREEMENT FOR THIS WORK.

PRELIMINARY PLANS  
 FOR REVIEW ONLY  
 NOT FOR CONSTRUCTION

| REV. | DATE | DESCRIPTION | BY |
|------|------|-------------|----|
|      |      |             |    |
|      |      |             |    |
|      |      |             |    |



CITY OF BENTONVILLE  
 BENTONVILLE, ARKANSAS  
 WALTON BOULEVARD  
 AND MEDICAL CENTER  
 PARKWAY IMPROVEMENTS

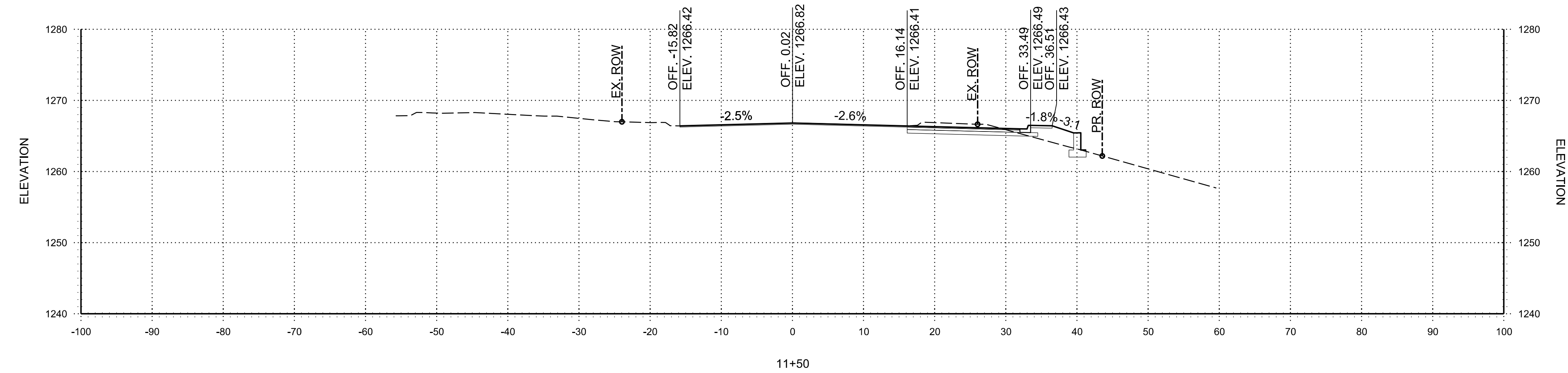
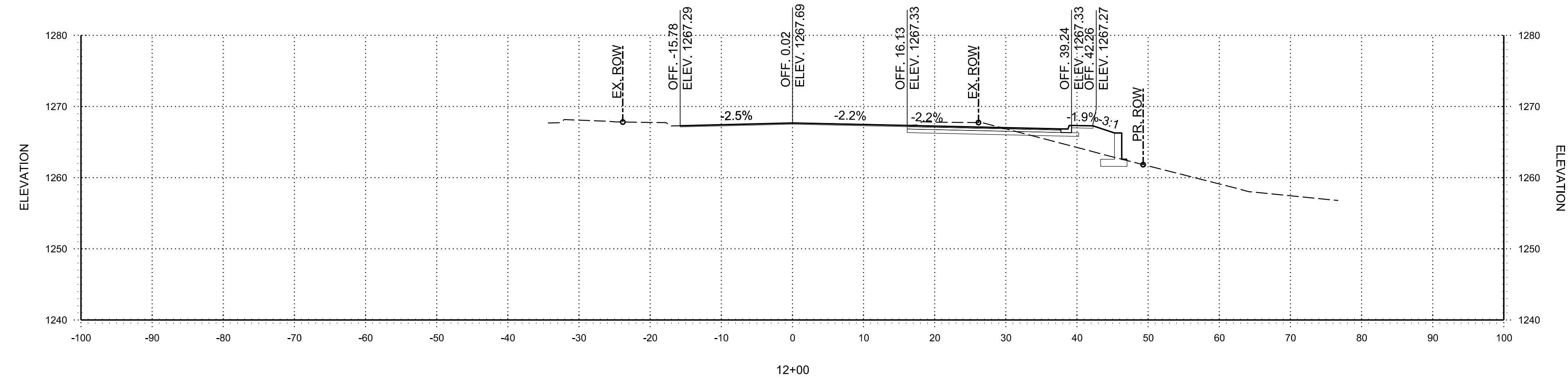
(S) MEDICAL CENTER  
 PARKWAY CROSS  
 SECTIONS

JOB NO.: 21T21080  
 DATE: JAN. 2022  
 DESIGNED BY: ZTM  
 DRAWN BY: TSA

BAR IS ONE INCH ON  
 ORIGINAL DRAWING  
 0 1" IF NOT ONE INCH ON THIS SHEET,  
 ADJUST SCALES ACCORDINGLY.

DRAWING NUMBER  
**X-101**

TSAUSTIN 1/26/2022 4:08:43 PM  
 WORKSPACE:Garver\_2012  
 Reviewing Data:Projects\2021\21T21080 - Bentonville Walton Blvd-SE Med Ctr Imps\Drawings\WMC\_X101\_CX.dgn  
 REVISION DATE:



STA. 11+50 TO STA. 12+00



© 2022 GARVER, LLC  
 THIS DOCUMENT, ALONG WITH THE  
 IDEAS AND DESIGNS CONVEYED  
 HEREIN, SHALL BE CONSIDERED  
 INSTRUMENTS OF PROFESSIONAL  
 SERVICE AND ARE PROPERTY OF  
 GARVER, LLC. ANY USE,  
 REPRODUCTION, OR DISTRIBUTION  
 OF THIS DOCUMENT, ALONG WITH  
 THE IDEAS AND DESIGN CONTAINED  
 HEREIN, IS PROHIBITED UNLESS  
 AUTHORIZED IN WRITING BY  
 GARVER, LLC OR EXPLICITLY  
 ALLOWED IN THE GOVERNING  
 PROFESSIONAL SERVICES  
 AGREEMENT FOR THIS WORK.

PRELIMINARY PLANS  
 FOR REVIEW ONLY  
 NOT FOR CONSTRUCTION

| REV. | DATE | DESCRIPTION | BY |
|------|------|-------------|----|
|      |      |             |    |



CITY OF BENTONVILLE  
 BENTONVILLE, ARKANSAS  
 WALTON BOULEVARD  
 AND MEDICAL CENTER  
 PARKWAY IMPROVEMENTS

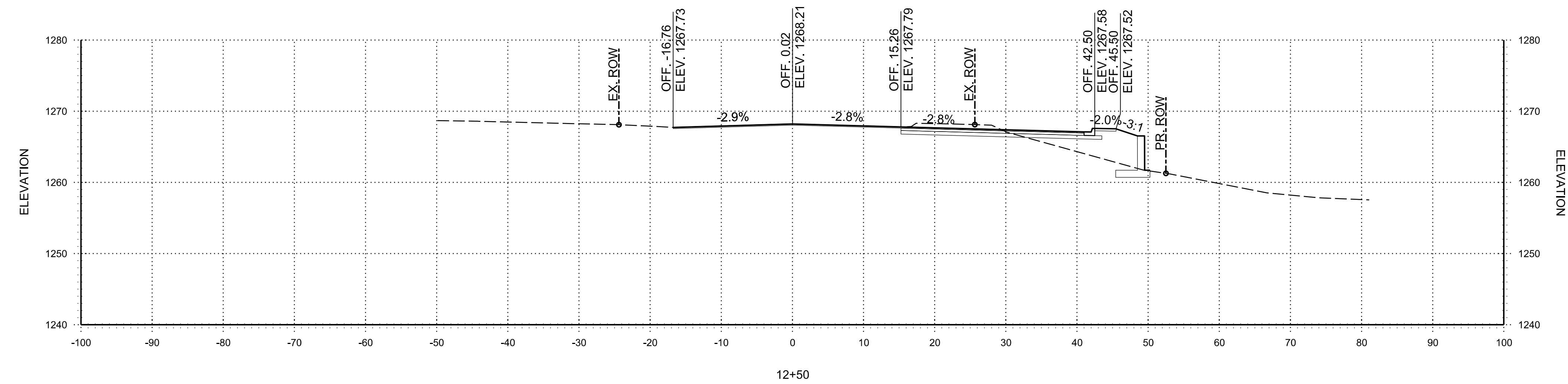
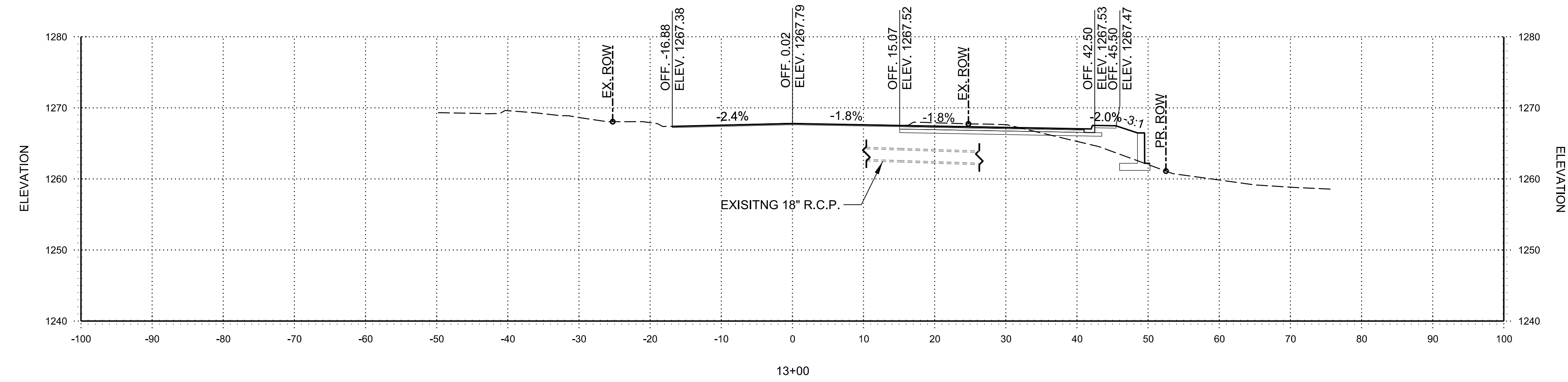
(S) MEDICAL CENTER  
 PARKWAY CROSS  
 SECTIONS

JOB NO.: 21T21080  
 DATE: JAN. 2022  
 DESIGNED BY: ZTM  
 DRAWN BY: TSA

BAR IS ONE INCH ON  
 ORIGINAL DRAWING  
 0 1"  
 IF NOT ONE INCH ON THIS SHEET,  
 ADJUST SCALES ACCORDINGLY.

DRAWING NUMBER  
**X-102**

TSAUSTIN 1/26/2022 4:08:44 PM  
 WORKSPACE:Garver\_2012  
 Reviewing Data:Projects\2021\21T21080 - Bentonville Walton Blvd-SE Med Ctr Imps\Drawings\WMC\_X101\_CX.dgn  
 REVISION DATE:



STA. 12+50 TO STA. 13+00



© 2022 GARVER, LLC  
 THIS DOCUMENT, ALONG WITH THE IDEAS AND DESIGNS CONVEYED HEREIN, SHALL BE CONSIDERED INSTRUMENTS OF PROFESSIONAL SERVICE AND ARE PROPERTY OF GARVER, LLC. ANY USE, REPRODUCTION, OR DISTRIBUTION OF THIS DOCUMENT, ALONG WITH THE IDEAS AND DESIGN CONTAINED HEREIN, IS PROHIBITED UNLESS AUTHORIZED IN WRITING BY GARVER, LLC OR EXPLICITLY ALLOWED IN THE GOVERNING PROFESSIONAL SERVICES AGREEMENT FOR THIS WORK.

PRELIMINARY PLANS  
 FOR REVIEW ONLY  
 NOT FOR CONSTRUCTION

| REV. | DATE | DESCRIPTION | BY |
|------|------|-------------|----|
|      |      |             |    |



CITY OF BENTONVILLE  
 BENTONVILLE, ARKANSAS  
 WALTON BOULEVARD  
 AND MEDICAL CENTER  
 PARKWAY IMPROVEMENTS

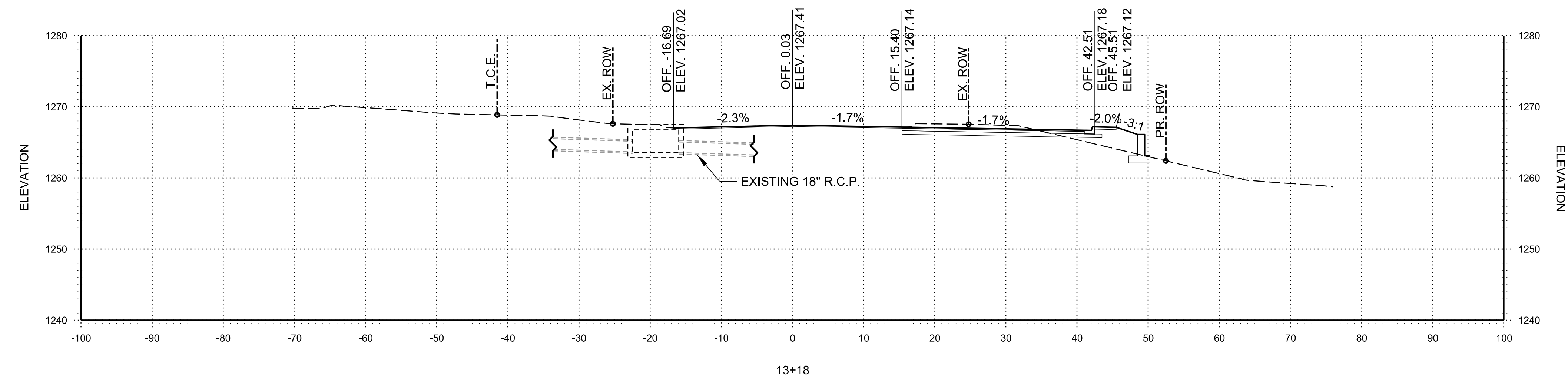
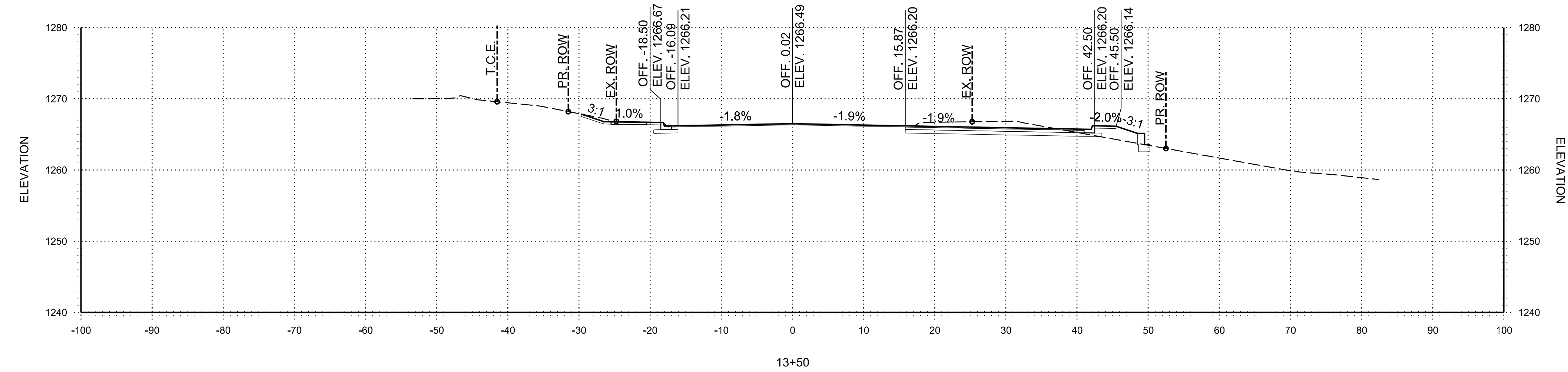
(S) MEDICAL CENTER  
 PARKWAY CROSS  
 SECTIONS

JOB NO.: 21T21080  
 DATE: JAN. 2022  
 DESIGNED BY: ZTM  
 DRAWN BY: TSA

BAR IS ONE INCH ON ORIGINAL DRAWING  
 0 1" IF NOT ONE INCH ON THIS SHEET, ADJUST SCALES ACCORDINGLY.

DRAWING NUMBER  
**X-103**

TSAUSTIN 1/26/2022 4:08:45 PM  
 WORKSPACE:Garver\_2012  
 Reviewing:Data\Projects\2021\21T21080 - Bentonville Walton Blvd-SE Med Ctr Imps\Drawings\WMC\_X101\_CX.dgn  
 REVISION DATE:



STA. 13+18 TO STA. 13+50



© 2022 GARVER, LLC  
 THIS DOCUMENT, ALONG WITH THE  
 IDEAS AND DESIGNS CONVEYED  
 HEREIN, SHALL BE CONSIDERED  
 INSTRUMENTS OF PROFESSIONAL  
 SERVICE AND ARE PROPERTY OF  
 GARVER, LLC. ANY USE,  
 REPRODUCTION, OR DISTRIBUTION  
 OF THIS DOCUMENT, ALONG WITH  
 THE IDEAS AND DESIGN CONTAINED  
 HEREIN, IS PROHIBITED UNLESS  
 AUTHORIZED IN WRITING BY  
 GARVER, LLC OR EXPLICITLY  
 ALLOWED IN THE GOVERNING  
 PROFESSIONAL SERVICES  
 AGREEMENT FOR THIS WORK.

PRELIMINARY PLANS  
 FOR REVIEW ONLY  
 NOT FOR CONSTRUCTION

| REV. | DATE | DESCRIPTION | BY |
|------|------|-------------|----|
|      |      |             |    |
|      |      |             |    |



CITY OF BENTONVILLE  
 BENTONVILLE, ARKANSAS

WALTON BOULEVARD  
 AND MEDICAL CENTER  
 PARKWAY IMPROVEMENTS

(S) MEDICAL CENTER  
 PARKWAY CROSS  
 SECTIONS

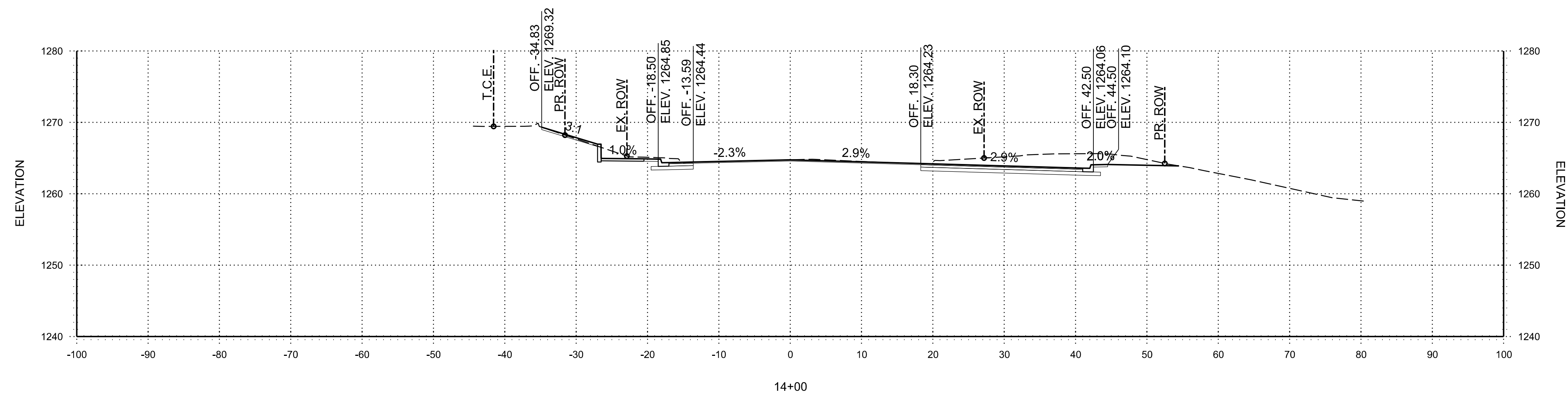
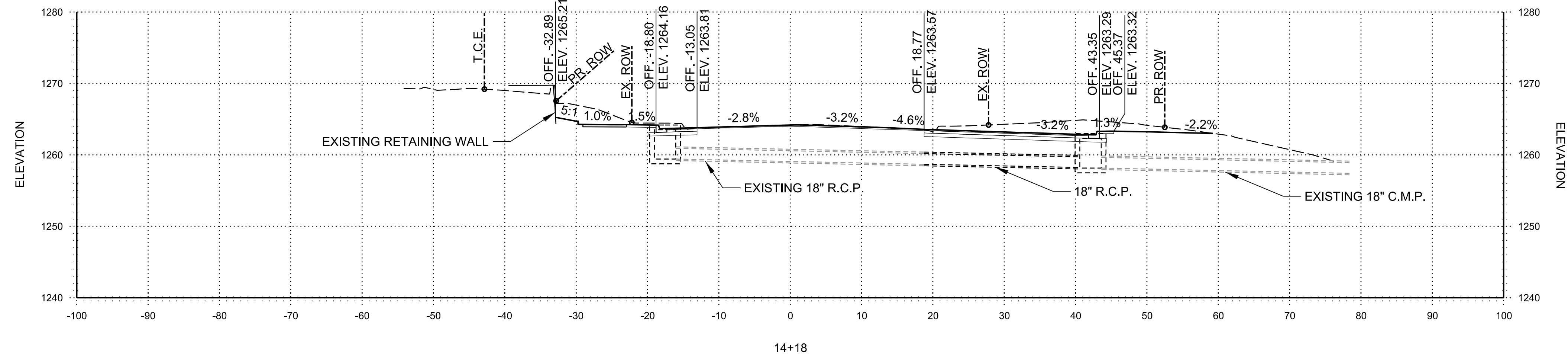
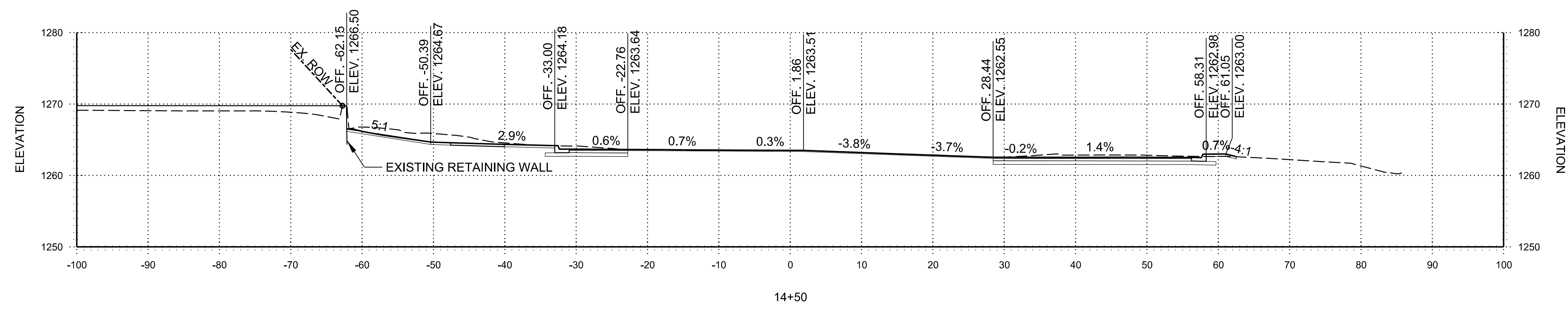
JOB NO.: 21T21080  
 DATE: JAN. 2022  
 DESIGNED BY: ZTM  
 DRAWN BY: TSA

BAR IS ONE INCH ON  
 ORIGINAL DRAWING  
 0 1" = 100'  
 IF NOT ONE INCH ON THIS SHEET,  
 ADJUST SCALES ACCORDINGLY.

DRAWING NUMBER  
**X-104**

TSAUSTIN 1/26/2022 4:08:45 PM  
 WORKSPACE:Garver\_2012  
 Revision: 01  
 Project: 202112171080 - Bentonville Walton Blvd-SE Med Ctr ImpsDrawingsWMC\_X101\_CX.dgn  
 REVISION DATE:

STA. 14+00 TO STA. 14+50



© 2022 GARVER, LLC  
 THIS DOCUMENT, ALONG WITH THE  
 IDEAS AND DESIGNS CONVEYED  
 HEREIN, SHALL BE CONSIDERED  
 INSTRUMENTS OF PROFESSIONAL  
 SERVICE AND ARE PROPERTY OF  
 GARVER, LLC. ANY USE,  
 REPRODUCTION, OR DISTRIBUTION  
 OF THIS DOCUMENT, ALONG WITH  
 THE IDEAS AND DESIGN CONTAINED  
 HEREIN, IS PROHIBITED UNLESS  
 AUTHORIZED IN WRITING BY  
 GARVER, LLC OR EXPLICITLY  
 ALLOWED IN THE GOVERNING  
 PROFESSIONAL SERVICES  
 AGREEMENT FOR THIS WORK.

PRELIMINARY PLANS  
 FOR REVIEW ONLY  
 NOT FOR CONSTRUCTION

| REV. | DATE | DESCRIPTION | BY |
|------|------|-------------|----|
|      |      |             |    |



CITY OF BENTONVILLE  
 BENTONVILLE, ARKANSAS  
 WALTON BOULEVARD  
 AND MEDICAL CENTER  
 PARKWAY IMPROVEMENTS

(S) MEDICAL CENTER  
 PARKWAY CROSS  
 SECTIONS

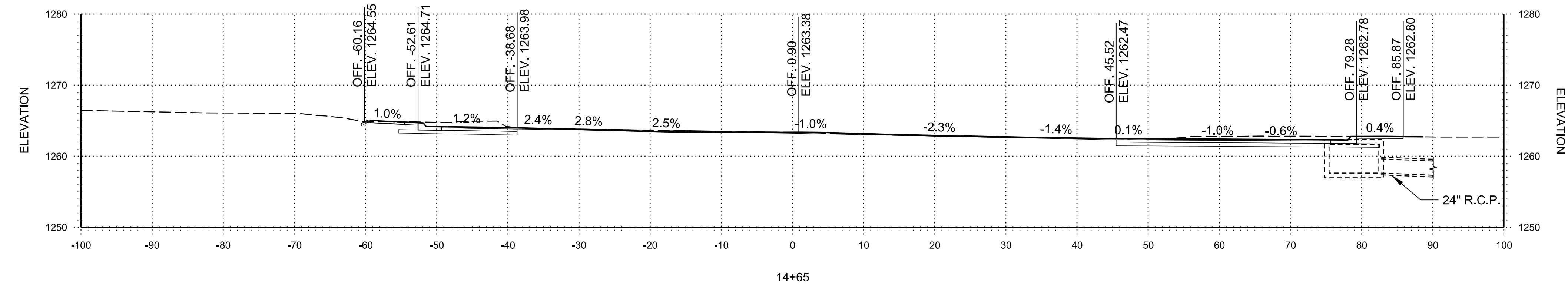
JOB NO.: 21T21080  
 DATE: JAN. 2022  
 DESIGNED BY: ZTM  
 DRAWN BY: TSA

BAR IS ONE INCH ON  
 ORIGINAL DRAWING  
 0 1" = 10'  
 IF NOT ONE INCH ON THIS SHEET,  
 ADJUST SCALES ACCORDINGLY.

DRAWING NUMBER  
**X-105**



TSAUSTIN 1/26/2022 4:08:46 PM  
 WORKSPACE:Garver\_2012  
 Review Date: 1/26/2022  
 Project: Bentonville Medical Center Parkway Cross Sections  
 Revised Date:



STA. 14+65 TO STA. 14+65

14+65



© 2022 GARVER, LLC  
 THIS DOCUMENT, ALONG WITH THE  
 IDEAS AND DESIGNS CONVEYED  
 HEREIN, SHALL BE CONSIDERED  
 INSTRUMENTS OF PROFESSIONAL  
 SERVICE AND ARE PROPERTY OF  
 GARVER, LLC. ANY USE,  
 REPRODUCTION, OR DISTRIBUTION  
 OF THIS DOCUMENT, ALONG WITH  
 THE IDEAS AND DESIGN CONTAINED  
 HEREIN, IS PROHIBITED UNLESS  
 AUTHORIZED IN WRITING BY  
 GARVER, LLC OR EXPLICITLY  
 ALLOWED IN THE GOVERNING  
 PROFESSIONAL SERVICES  
 AGREEMENT FOR THIS WORK.

PRELIMINARY PLANS  
 FOR REVIEW ONLY  
 NOT FOR CONSTRUCTION

| REV. | DATE | DESCRIPTION | BY |
|------|------|-------------|----|
|      |      |             |    |
|      |      |             |    |
|      |      |             |    |



CITY OF BENTONVILLE  
 BENTONVILLE, ARKANSAS  
 WALTON BOULEVARD  
 AND MEDICAL CENTER  
 PARKWAY IMPROVEMENTS

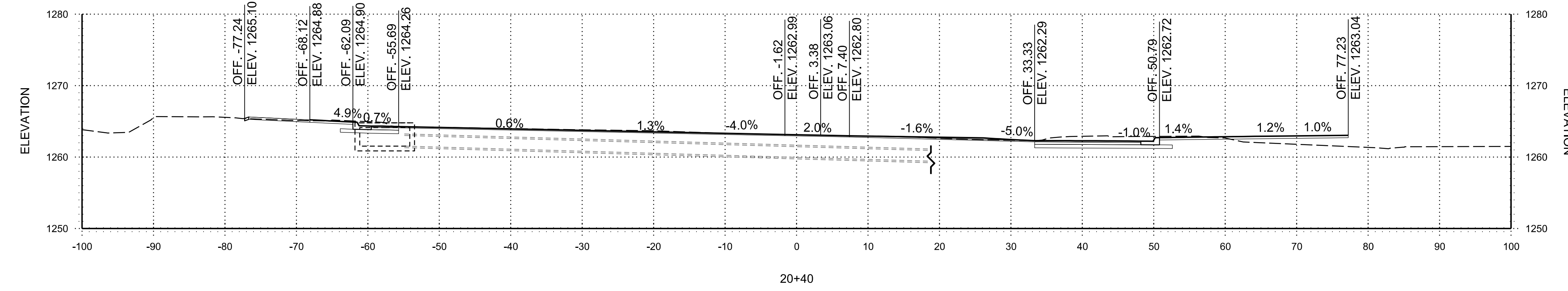
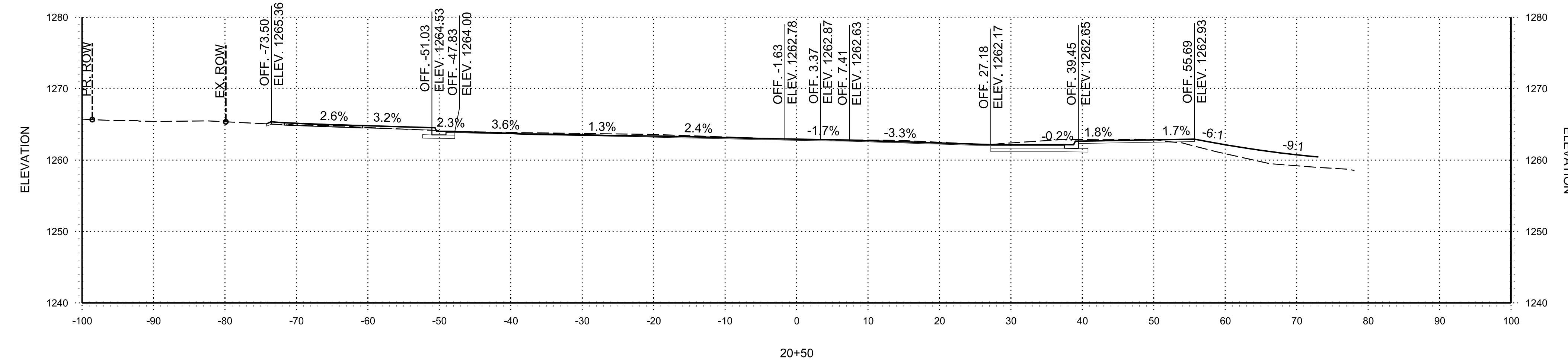
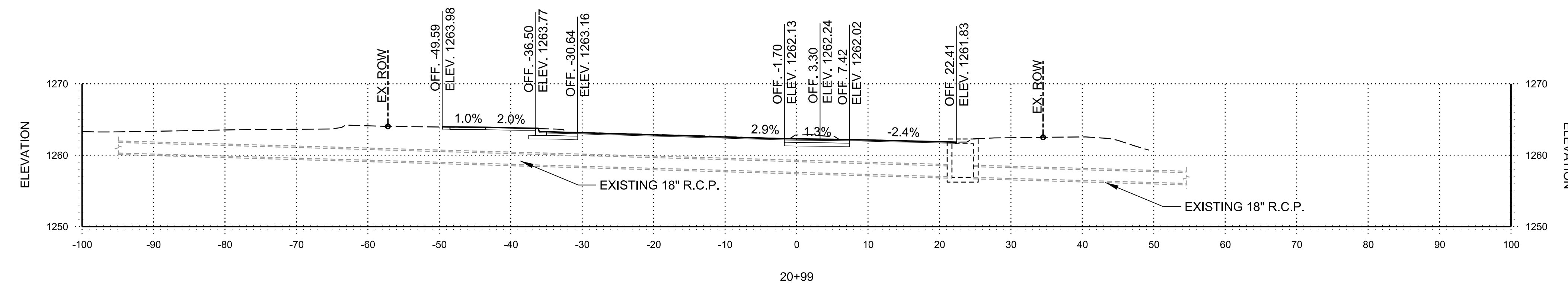
(S) MEDICAL CENTER  
 PARKWAY CROSS  
 SECTIONS

JOB NO.: 21T21080  
 DATE: JAN. 2022  
 DESIGNED BY: ZTM  
 DRAWN BY: TSA

BAR IS ONE INCH ON  
 ORIGINAL DRAWING  
 0 1" = 100'  
 IF NOT ONE INCH ON THIS SHEET,  
 ADJUST SCALES ACCORDINGLY.

DRAWING NUMBER  
**X-106**

TSAUSTIN 1/26/2022 4:08:47 PM  
 WORKSPACE:Garver\_2012  
 Revision:202112171080 - Bentonville Walton Blvd-SE Med Ctr ImpsDrawings\WMC\_X101\_CX.dgn  
 REVISION DATE:



STA. 20+40 TO STA. 20+99



© 2022 GARVER, LLC  
 THIS DOCUMENT, ALONG WITH THE  
 IDEAS AND DESIGNS CONVEYED  
 HEREIN, SHALL BE CONSIDERED  
 INSTRUMENTS OF PROFESSIONAL  
 SERVICE AND ARE PROPERTY OF  
 GARVER, LLC. ANY USE,  
 REPRODUCTION, OR DISTRIBUTION  
 OF THIS DOCUMENT, ALONG WITH  
 THE IDEAS AND DESIGN CONTAINED  
 HEREIN, IS PROHIBITED UNLESS  
 AUTHORIZED IN WRITING BY  
 GARVER, LLC OR EXPLICITLY  
 ALLOWED IN THE GOVERNING  
 PROFESSIONAL SERVICES  
 AGREEMENT FOR THIS WORK.

PRELIMINARY PLANS  
 FOR REVIEW ONLY  
 NOT FOR CONSTRUCTION

| REV. | DATE | DESCRIPTION | BY |
|------|------|-------------|----|
|      |      |             |    |
|      |      |             |    |
|      |      |             |    |



CITY OF BENTONVILLE  
 BENTONVILLE, ARKANSAS  
 WALTON BOULEVARD  
 AND MEDICAL CENTER  
 PARKWAY IMPROVEMENTS

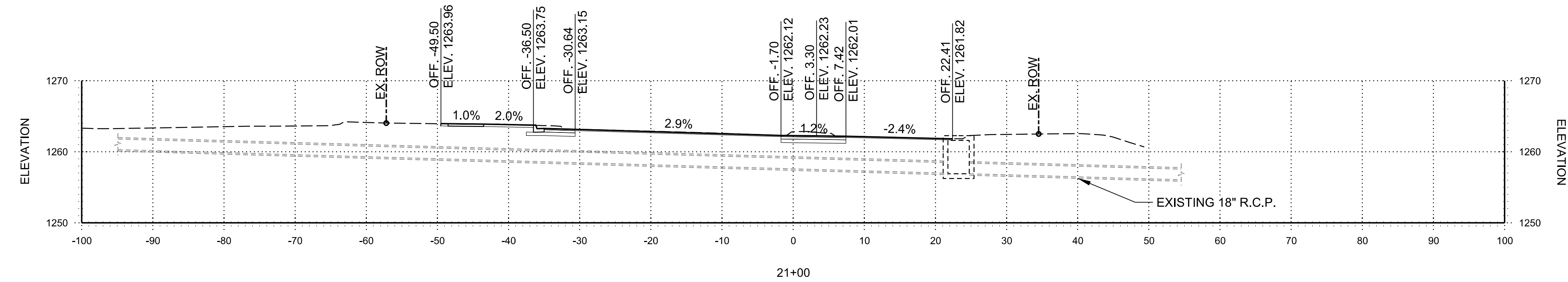
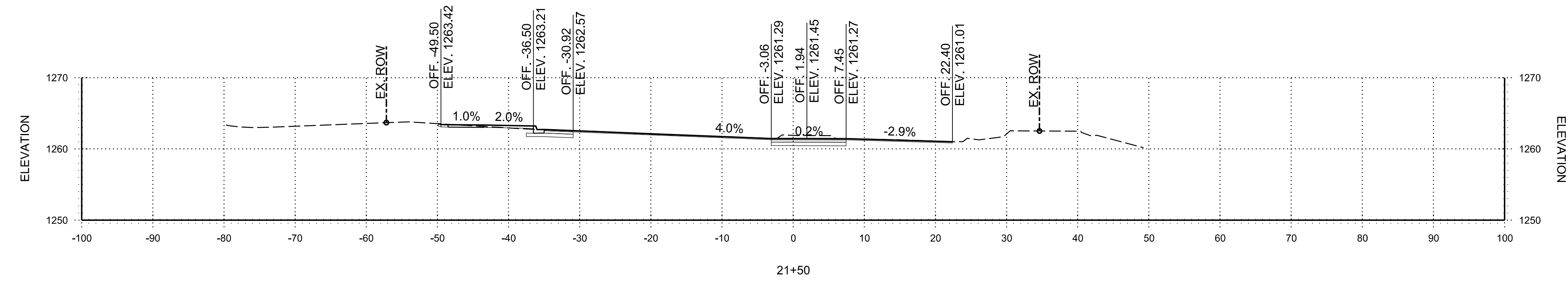
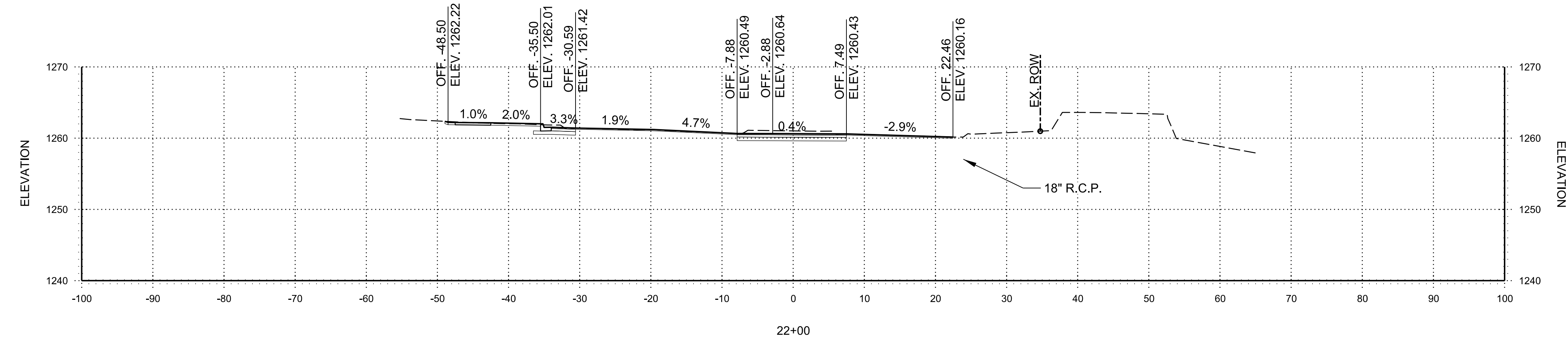
(N) MEDICAL CENTER  
 PARKWAY CROSS  
 SECTIONS

JOB NO.: 21T21080  
 DATE: JAN. 2022  
 DESIGNED BY: ZTM  
 DRAWN BY: TSA

BAR IS ONE INCH ON  
 ORIGINAL DRAWING  
 0 1" = 100'  
 IF NOT ONE INCH ON THIS SHEET,  
 ADJUST SCALES ACCORDINGLY.

DRAWING NUMBER  
**X-201**

TSAUSTIN 1/26/2022 4:08:47 PM  
 WORKSPACE:Garver\_2012  
 Reviewing Data:Projects\2021\21080 - Bentonville Walton Blvd-SE Med Ctr Imps\Drawings\WMC\_X101\_CX.dgn  
 REVISION DATE:



STA. 21+00 TO STA. 22+00



© 2022 GARVER, LLC  
 THIS DOCUMENT, ALONG WITH THE  
 IDEAS AND DESIGNS CONVEYED  
 HEREIN, SHALL BE CONSIDERED  
 INSTRUMENTS OF PROFESSIONAL  
 SERVICE AND ARE PROPERTY OF  
 GARVER, LLC. ANY USE,  
 REPRODUCTION, OR DISTRIBUTION  
 OF THIS DOCUMENT, ALONG WITH  
 THE IDEAS AND DESIGN CONTAINED  
 HEREIN, IS PROHIBITED UNLESS  
 AUTHORIZED IN WRITING BY  
 GARVER, LLC OR EXPLICITLY  
 ALLOWED IN THE GOVERNING  
 PROFESSIONAL SERVICES  
 AGREEMENT FOR THIS WORK.

PRELIMINARY PLANS  
 FOR REVIEW ONLY  
 NOT FOR CONSTRUCTION

| REV. | DATE | DESCRIPTION | BY |
|------|------|-------------|----|
|      |      |             |    |
|      |      |             |    |
|      |      |             |    |



CITY OF BENTONVILLE  
 BENTONVILLE, ARKANSAS  
 WALTON BOULEVARD  
 AND MEDICAL CENTER  
 PARKWAY IMPROVEMENTS

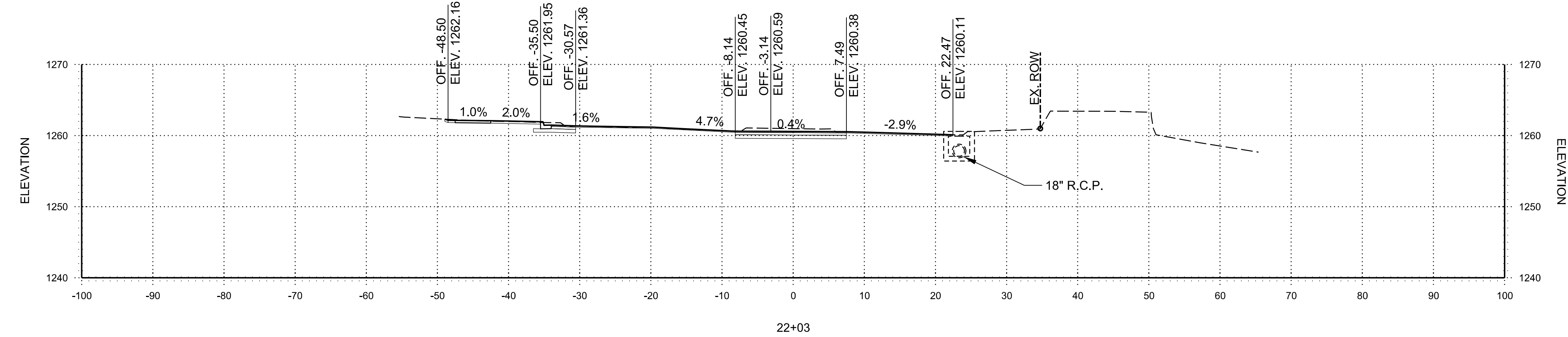
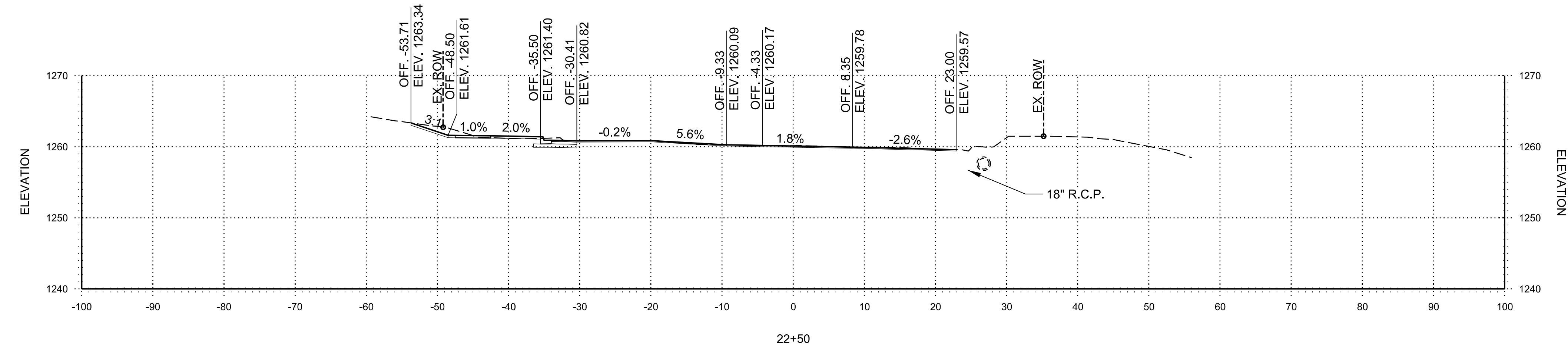
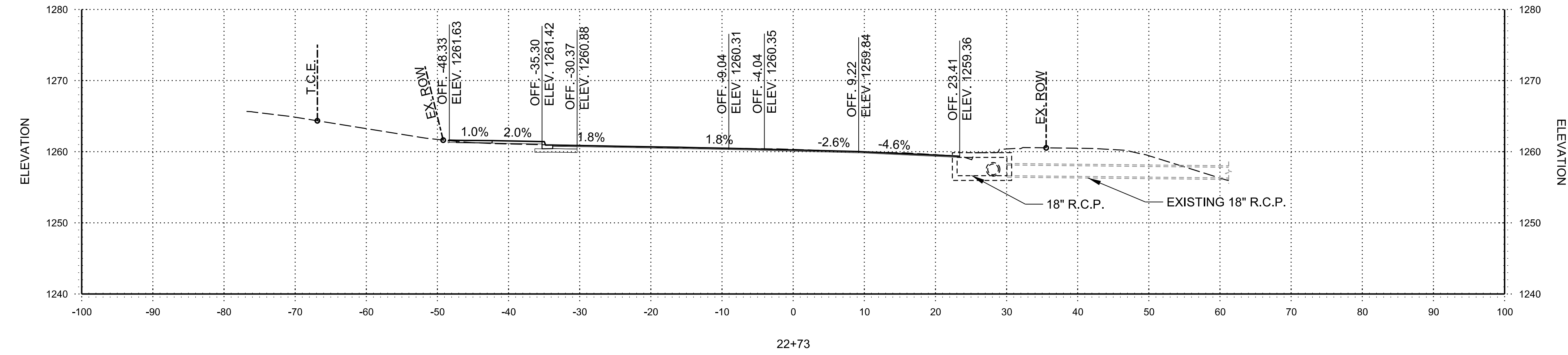
(N) MEDICAL CENTER  
 PARKWAY CROSS  
 SECTIONS

JOB NO.: 21T21080  
 DATE: JAN. 2022  
 DESIGNED BY: ZTM  
 DRAWN BY: TSA

BAR IS ONE INCH ON  
 ORIGINAL DRAWING  
 0 1" = 100'  
 IF NOT ONE INCH ON THIS SHEET,  
 ADJUST SCALES ACCORDINGLY.

DRAWING NUMBER  
**X-202**

TSAUSTIN 1/26/2022 4:08:48 PM  
 WORKSPACE:Garver\_2012  
 \\garver-bent\shared\Projects\2021\21T21080 - Bentonville Walton Blvd-SE Med Ctr Imps\Drawings\WMC\_X101\_CX.dgn  
 REVISION DATE:



STA. 22+03 TO STA. 22+73



© 2022 GARVER, LLC  
 THIS DOCUMENT, ALONG WITH THE  
 IDEAS AND DESIGNS CONVEYED  
 HEREIN, SHALL BE CONSIDERED  
 INSTRUMENTS OF PROFESSIONAL  
 SERVICE AND ARE PROPERTY OF  
 GARVER, LLC. ANY USE,  
 REPRODUCTION, OR DISTRIBUTION  
 OF THIS DOCUMENT, ALONG WITH  
 THE IDEAS AND DESIGN CONTAINED  
 HEREIN, IS PROHIBITED UNLESS  
 AUTHORIZED IN WRITING BY  
 GARVER, LLC OR EXPLICITLY  
 ALLOWED IN THE GOVERNING  
 PROFESSIONAL SERVICES  
 AGREEMENT FOR THIS WORK.

PRELIMINARY PLANS  
 FOR REVIEW ONLY  
 NOT FOR CONSTRUCTION

| REV. | DATE | DESCRIPTION | BY |
|------|------|-------------|----|
|      |      |             |    |
|      |      |             |    |
|      |      |             |    |



CITY OF BENTONVILLE  
 BENTONVILLE, ARKANSAS  
 WALTON BOULEVARD  
 AND MEDICAL CENTER  
 PARKWAY IMPROVEMENTS

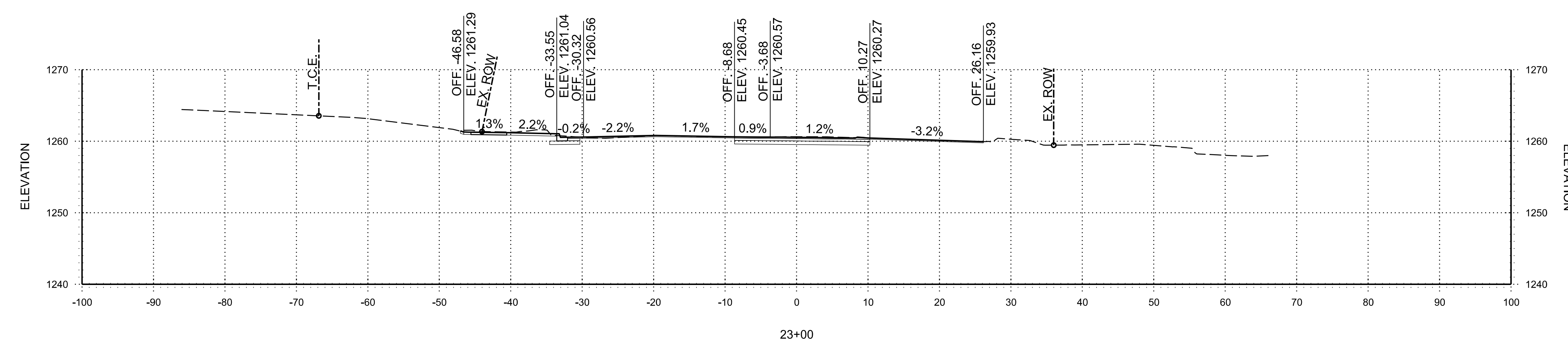
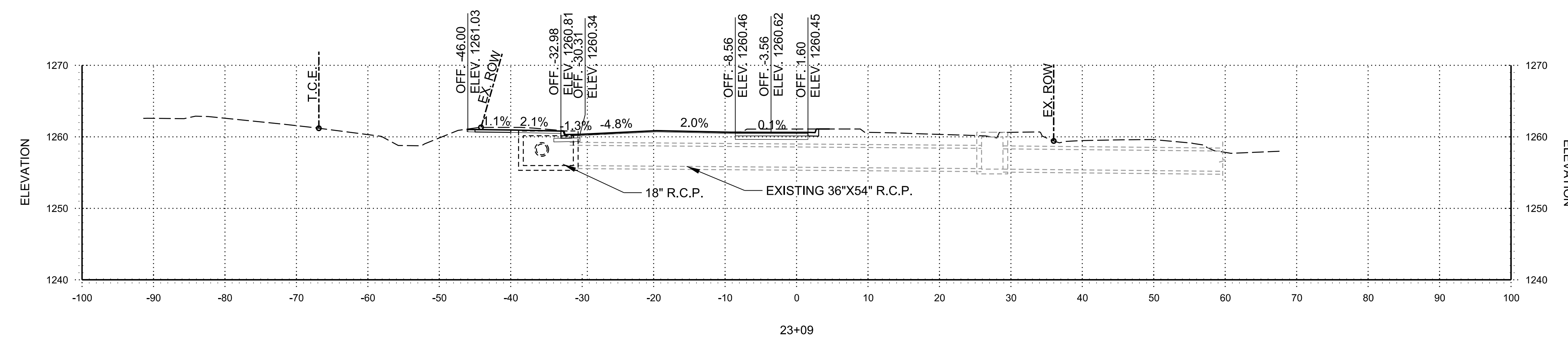
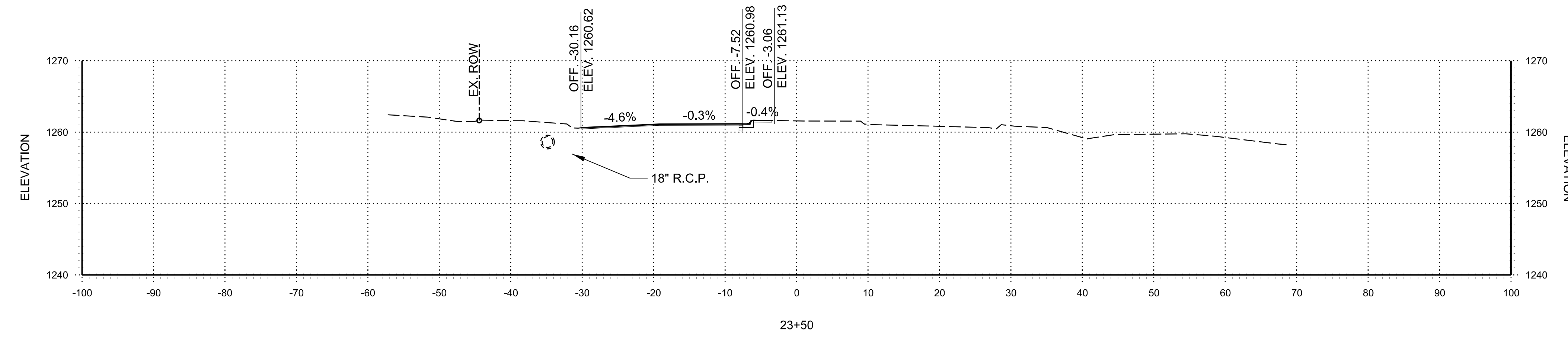
(N) MEDICAL CENTER  
 PARKWAY CROSS  
 SECTIONS

JOB NO.: 21T21080  
 DATE: JAN. 2022  
 DESIGNED BY: ZTM  
 DRAWN BY: TSA

BAR IS ONE INCH ON  
 ORIGINAL DRAWING  
 0 1" IF NOT ONE INCH ON THIS SHEET,  
 ADJUST SCALES ACCORDINGLY.

DRAWING NUMBER  
**X-203**

TSAUSTIN 1/26/2022 4:08:49 PM  
 WORKSPACE:Garver\_2012  
 Reviewing:Garver\_2012  
 Project:W:\data\Projects\2021\21T21080 - Bentonville Walton Blvd-SE Med Ctr Imps\Drawings\WMC\_X101\_CX.dgn  
 REVISION DATE:



STA. 23+00 TO STA. 23+50



© 2022 GARVER, LLC  
 THIS DOCUMENT, ALONG WITH THE  
 IDEAS AND DESIGNS CONVEYED  
 HEREIN, SHALL BE CONSIDERED  
 INSTRUMENTS OF PROFESSIONAL  
 SERVICE AND ARE PROPERTY OF  
 GARVER, LLC. ANY USE,  
 REPRODUCTION, OR DISTRIBUTION  
 OF THIS DOCUMENT, ALONG WITH  
 THE IDEAS AND DESIGN CONTAINED  
 HEREIN, IS PROHIBITED UNLESS  
 AUTHORIZED IN WRITING BY  
 GARVER, LLC OR EXPLICITLY  
 ALLOWED IN THE GOVERNING  
 PROFESSIONAL SERVICE  
 AGREEMENT FOR THIS WORK.

PRELIMINARY PLANS  
 FOR REVIEW ONLY  
 NOT FOR CONSTRUCTION

| REV. | DATE | DESCRIPTION | BY |
|------|------|-------------|----|
|      |      |             |    |
|      |      |             |    |
|      |      |             |    |



CITY OF BENTONVILLE  
 BENTONVILLE, ARKANSAS  
 WALTON BOULEVARD  
 AND MEDICAL CENTER  
 PARKWAY IMPROVEMENTS

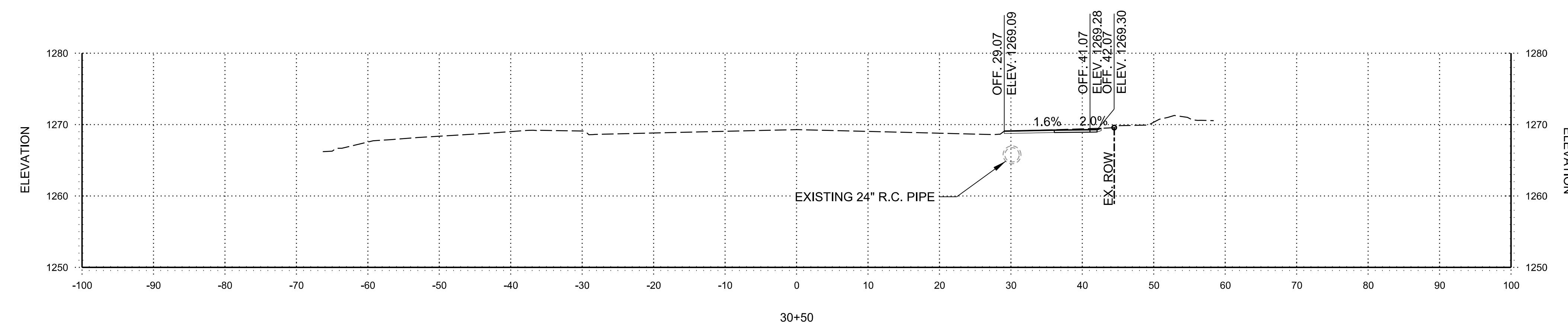
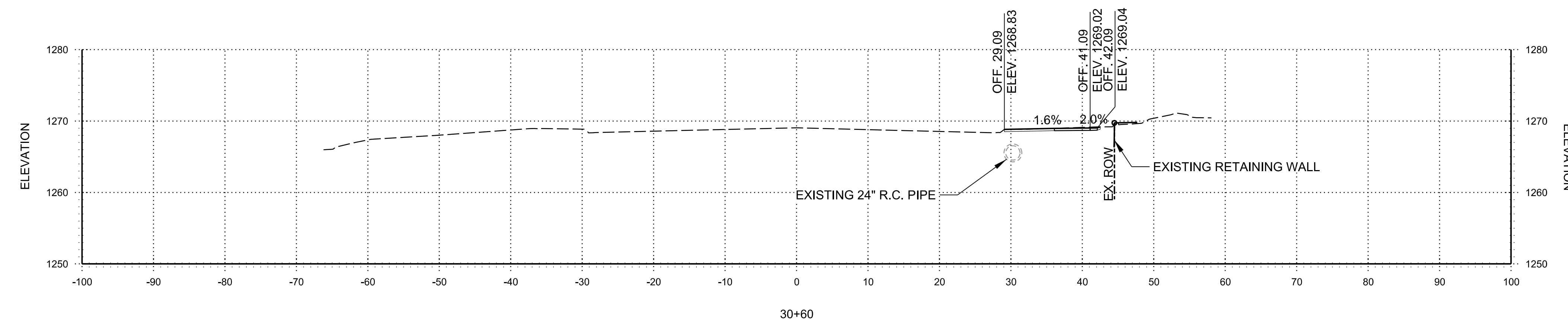
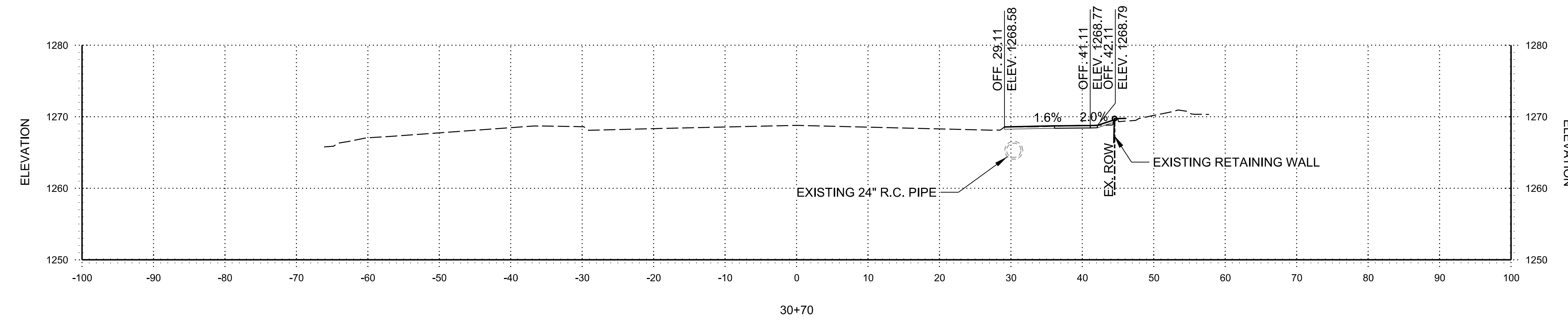
(N) MEDICAL CENTER  
 PARKWAY CROSS  
 SECTIONS

JOB NO.: 21T21080  
 DATE: JAN. 2022  
 DESIGNED BY: ZTM  
 DRAWN BY: TSA

BAR IS ONE INCH ON  
 ORIGINAL DRAWING  
 0 1"  
 IF NOT ONE INCH ON THIS SHEET,  
 ADJUST SCALES ACCORDINGLY.

DRAWING NUMBER  
**X-204**

TSAUSTIN 1/26/2022 4:08:50 PM  
 WORKSPACE:Garver\_2012  
 Review Date: 1/26/2022  
 REVISION DATE:  
 Review Date: 1/26/2022  
 Bentley ProjectWise  
 - Bentonville - Walton Blvd-SE Med Ctr Imps\Drawings\WMC\_X101\_CX.dgn



STA. 30+50 TO STA. 30+70



© 2022 GARVER, LLC  
 THIS DOCUMENT, ALONG WITH THE  
 IDEAS AND DESIGNS CONVEYED  
 HEREIN, SHALL BE CONSIDERED  
 INSTRUMENTS OF PROFESSIONAL  
 SERVICE AND ARE PROPERTY OF  
 GARVER, LLC. ANY USE,  
 REPRODUCTION, OR DISTRIBUTION  
 OF THIS DOCUMENT, ALONG WITH  
 THE IDEAS AND DESIGN CONTAINED  
 HEREIN, IS PROHIBITED UNLESS  
 AUTHORIZED IN WRITING BY  
 GARVER, LLC OR EXPLICITLY  
 ALLOWED IN THE GOVERNING  
 PROFESSIONAL SERVICE  
 AGREEMENT FOR THIS WORK.

PRELIMINARY PLANS  
 FOR REVIEW ONLY  
 NOT FOR CONSTRUCTION

| REV. | DATE | DESCRIPTION | BY |
|------|------|-------------|----|
|      |      |             |    |
|      |      |             |    |
|      |      |             |    |



CITY OF BENTONVILLE  
 BENTONVILLE, ARKANSAS  
 WALTON BOULEVARD  
 AND MEDICAL CENTER  
 PARKWAY IMPROVEMENTS

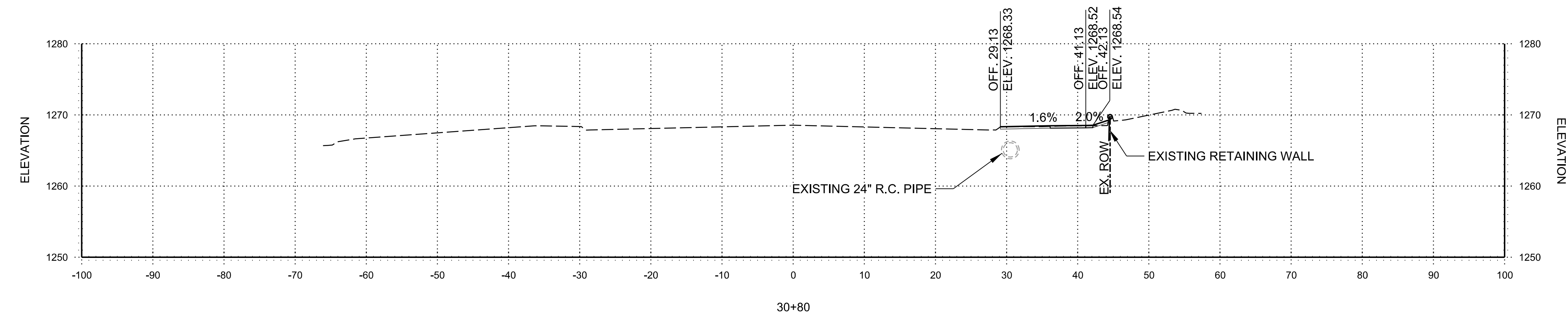
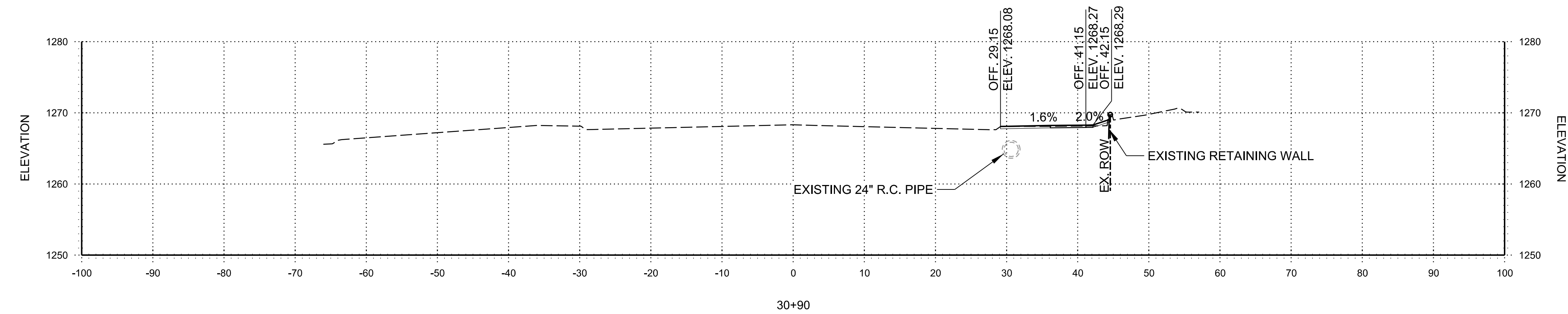
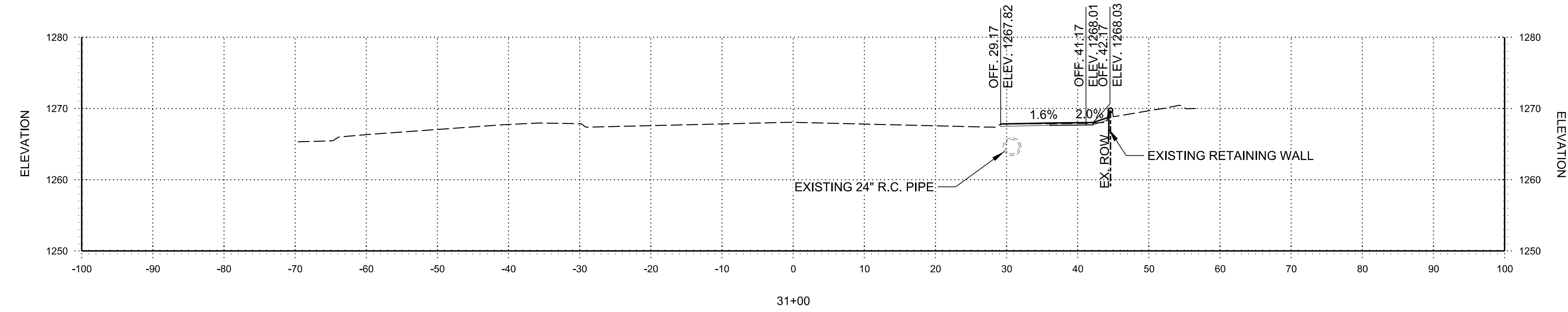
SE WALTON  
 BOULEVARD CROSS  
 SECTIONS

JOB NO.: 21T21080  
 DATE: JAN. 2022  
 DESIGNED BY: ZTM  
 DRAWN BY: TSA

BAR IS ONE INCH ON  
 ORIGINAL DRAWING  
 0 1" = 100'  
 IF NOT ONE INCH ON THIS SHEET,  
 ADJUST SCALES ACCORDINGLY.

DRAWING NUMBER  
**X-301**

TSAUSTIN 1/26/2022 4:08:50 PM  
 WORKSPACE:Garver\_2012  
 Reviewing Data:Projects\2021\21T21080 - Bentonville Walton Blvd-SE Med Ctr Imps\Drawings\WMC\_X101\_CX.dgn  
 REVISION DATE:



STA. 30+80 TO STA. 31+00



© 2022 GARVER, LLC  
 THIS DOCUMENT, ALONG WITH THE IDEAS AND DESIGNS CONVEYED HEREIN, SHALL BE CONSIDERED INSTRUMENTS OF PROFESSIONAL SERVICE AND ARE PROPERTY OF GARVER, LLC. ANY USE, REPRODUCTION, OR DISTRIBUTION OF THIS DOCUMENT, ALONG WITH THE IDEAS AND DESIGN CONTAINED HEREIN, IS PROHIBITED UNLESS AUTHORIZED IN WRITING BY GARVER, LLC OR EXPLICITLY ALLOWED IN THE GOVERNING PROFESSIONAL SERVICES AGREEMENT FOR THIS WORK.

PRELIMINARY PLANS  
 FOR REVIEW ONLY  
 NOT FOR CONSTRUCTION

| REV. | DATE | DESCRIPTION | BY |
|------|------|-------------|----|
|      |      |             |    |
|      |      |             |    |
|      |      |             |    |



CITY OF BENTONVILLE  
 BENTONVILLE, ARKANSAS  
 WALTON BOULEVARD  
 AND MEDICAL CENTER  
 PARKWAY IMPROVEMENTS

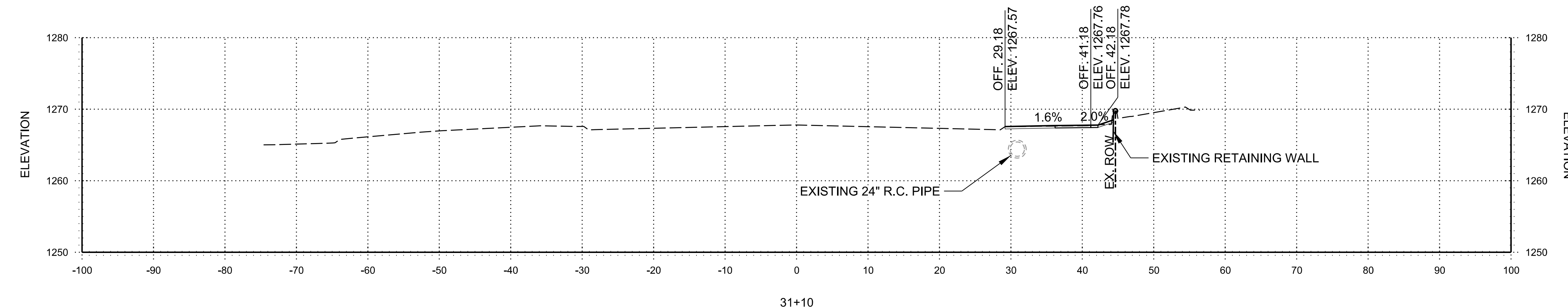
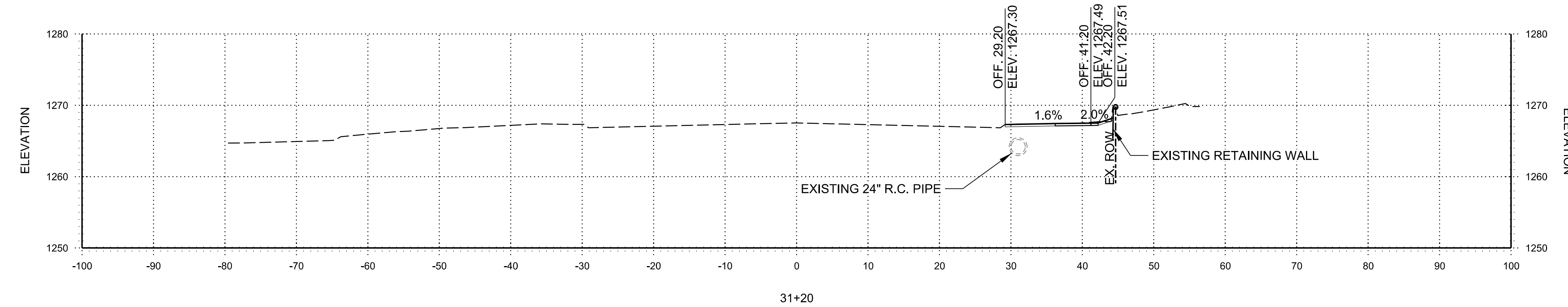
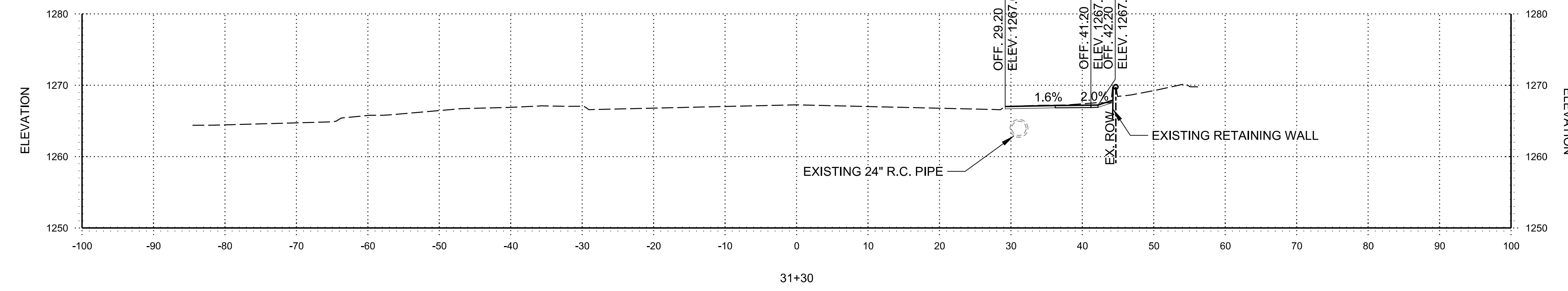
SE WALTON  
 BOULEVARD CROSS  
 SECTIONS

JOB NO.: 21T21080  
 DATE: JAN. 2022  
 DESIGNED BY: ZTM  
 DRAWN BY: TSA

BAR IS ONE INCH ON ORIGINAL DRAWING  
 0 1" IF NOT ONE INCH ON THIS SHEET, ADJUST SCALES ACCORDINGLY.

DRAWING NUMBER  
**X-302**

TSAUSTIN 1/26/2022 4:08:51 PM  
 WORKSPACE:Garver\_2012  
 Reviewing Data:Projects\2021\21T21080 - Bentonville Walton Blvd-SE Med Ctr Imps\Drawings\WMC\_X101\_CX.dgn  
 REVISION DATE:



STA. 31+10 TO STA. 31+30



© 2022 GARVER, LLC  
 THIS DOCUMENT, ALONG WITH THE IDEAS AND DESIGNS CONVEYED HEREIN, SHALL BE CONSIDERED INSTRUMENTS OF PROFESSIONAL SERVICE AND ARE PROPERTY OF GARVER, LLC. ANY USE, REPRODUCTION, OR DISTRIBUTION OF THIS DOCUMENT, ALONG WITH THE IDEAS AND DESIGN CONTAINED HEREIN, IS PROHIBITED UNLESS AUTHORIZED IN WRITING BY GARVER, LLC OR EXPLICITLY ALLOWED IN THE GOVERNING PROFESSIONAL SERVICE AGREEMENT FOR THIS WORK.

PRELIMINARY PLANS  
 FOR REVIEW ONLY  
 NOT FOR CONSTRUCTION

| REV. | DATE | DESCRIPTION | BY |
|------|------|-------------|----|
|      |      |             |    |
|      |      |             |    |



CITY OF BENTONVILLE  
 BENTONVILLE, ARKANSAS  
 WALTON BOULEVARD  
 AND MEDICAL CENTER  
 PARKWAY IMPROVEMENTS

SE WALTON  
 BOULEVARD CROSS  
 SECTIONS

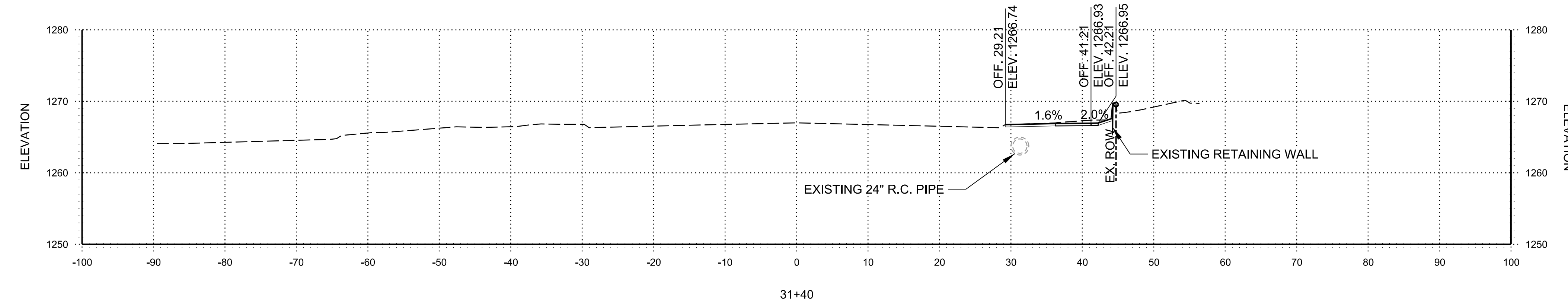
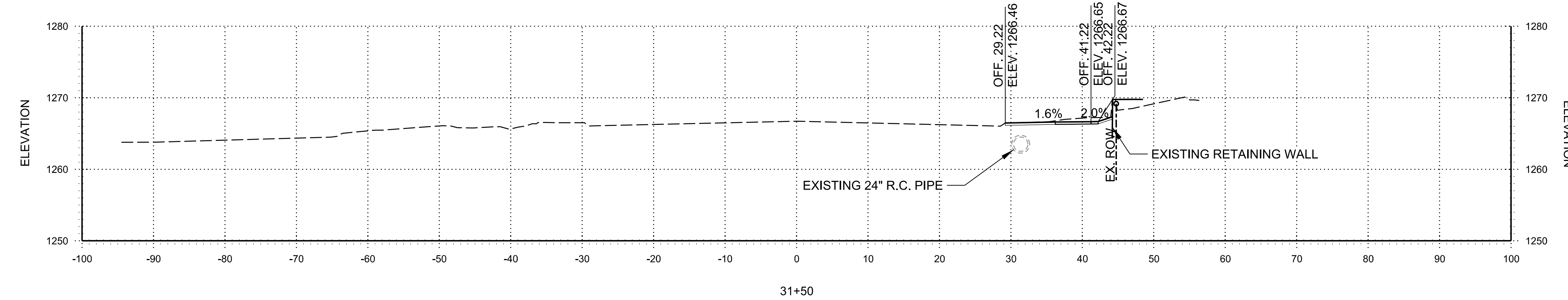
JOB NO.: 21T21080  
 DATE: JAN. 2022  
 DESIGNED BY: ZTM  
 DRAWN BY: TSA

BAR IS ONE INCH ON ORIGINAL DRAWING  
 0 1" IF NOT ONE INCH ON THIS SHEET, ADJUST SCALES ACCORDINGLY.

DRAWING NUMBER  
**X-303**



TSAUSTIN 1/26/2022 4:08:52 PM  
 WORKSPACE:Garver\_2012  
 Review Date: 1/26/2022  
 REVISION DATE:  
 Bentley ProjectWise  
 - Bentonville Walton Blvd-SE Med Ctr Imps\Drawings\WMC\_X101\_CX.dgn



STA. 31+40 TO STA. 31+50



© 2022 GARVER, LLC  
 THIS DOCUMENT, ALONG WITH THE  
 IDEAS AND DESIGNS CONVEYED  
 HEREIN, SHALL BE CONSIDERED  
 INSTRUMENTS OF PROFESSIONAL  
 SERVICE AND ARE PROPERTY OF  
 GARVER, LLC. ANY USE,  
 REPRODUCTION, OR DISTRIBUTION  
 OF THIS DOCUMENT, ALONG WITH  
 THE IDEAS AND DESIGN CONTAINED  
 HEREIN, IS PROHIBITED UNLESS  
 AUTHORIZED IN WRITING BY  
 GARVER, LLC OR EXPLICITLY  
 ALLOWED IN THE GOVERNING  
 PROFESSIONAL SERVICES  
 AGREEMENT FOR THIS WORK.

PRELIMINARY PLANS  
 FOR REVIEW ONLY  
 NOT FOR CONSTRUCTION

| REV. | DATE | DESCRIPTION | BY |
|------|------|-------------|----|
|      |      |             |    |
|      |      |             |    |
|      |      |             |    |



CITY OF BENTONVILLE  
 BENTONVILLE, ARKANSAS  
 WALTON BOULEVARD  
 AND MEDICAL CENTER  
 PARKWAY IMPROVEMENTS

SE WALTON  
 BOULEVARD CROSS  
 SECTIONS

JOB NO.: 21T21080  
 DATE: JAN. 2022  
 DESIGNED BY: ZTM  
 DRAWN BY: TSA

BAR IS ONE INCH ON  
 ORIGINAL DRAWING  
 0 1"  
 IF NOT ONE INCH ON THIS SHEET,  
 ADJUST SCALES ACCORDINGLY.

DRAWING NUMBER  
**X-304**